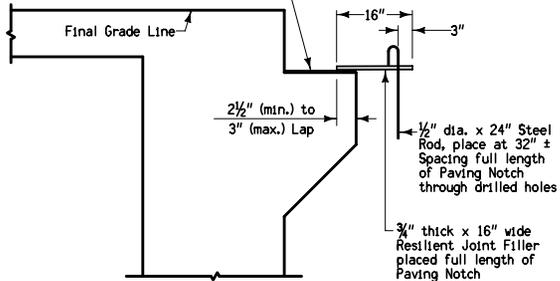


Debond paving notch with two (2) layers of 30# asphaltic felt paper full length of Paving Notch



DETAIL 'B'
(Moveable Abutment)

JOINT TYPE FOR MOVEABLE ABUTMENT BRIDGES		
Joint	Maximum Bridge Length	
	Concrete Beam or Slab	Steel Girder
CF-1	370'	250'
CF-2	465'	320'
CF-3	575'	400'

For joint details, see PV-1.

All Transverse Bars are #5.

- ① 2" to 2 1/2" clear to bent bar.
- ② Minimum lap length: #5 bars - 18 inches
#6 bars - 27 inches
#8 bars - 48 inches
- ③ If bridge is skewed, place additional #5 bar parallel to skewed face.

For curb details, see PV-2

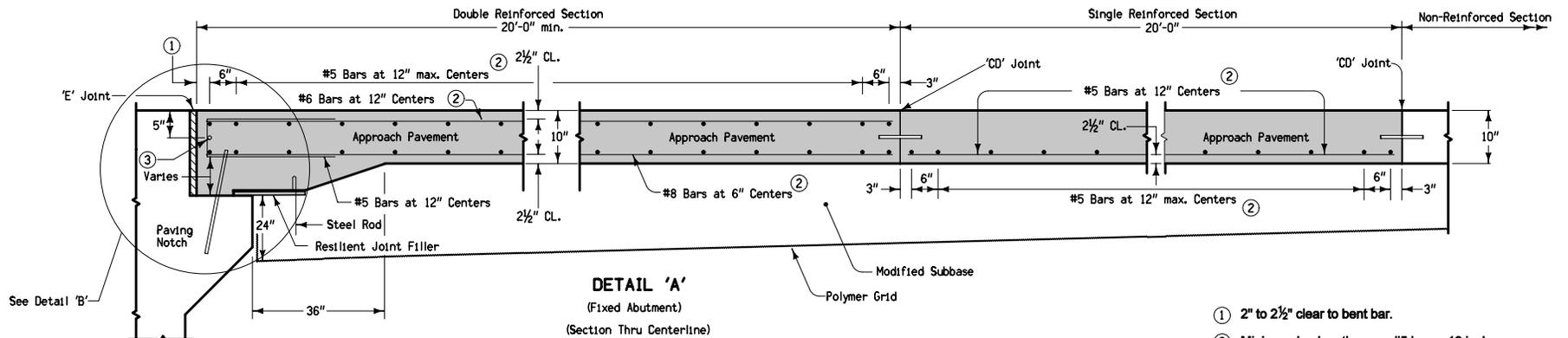
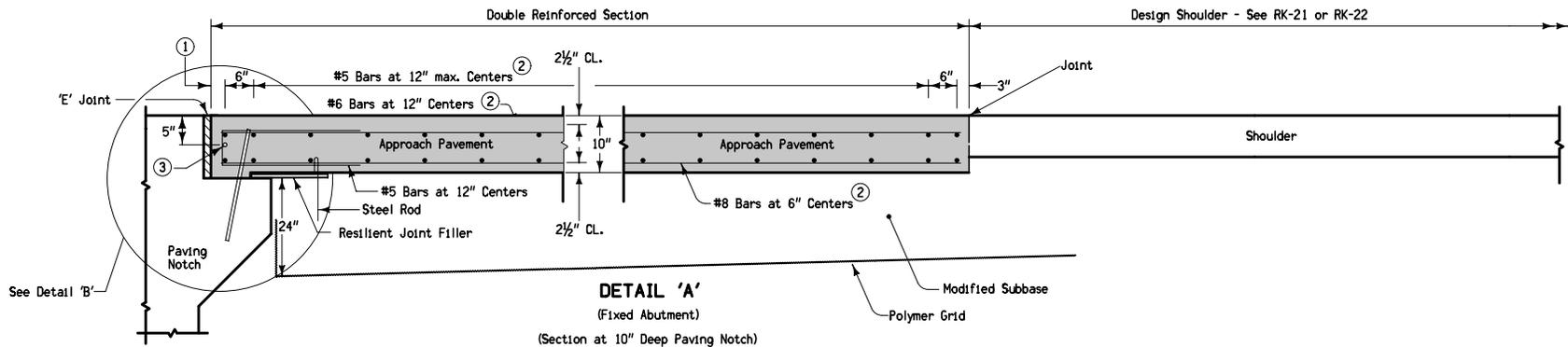
Possible Contract Item:

Bridge Approach, RK-26

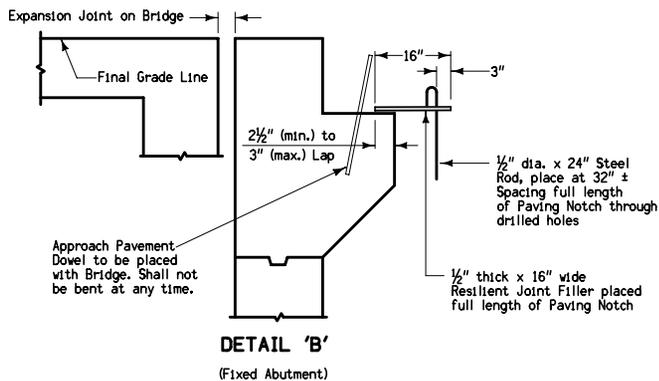
Possible Tabulation:

112-6

 Iowa Department of Transportation	REVISION
	5 10-19-10
STANDARD ROAD PLAN	RK-26
SHEET 1 of 4	
REVISIONS: Changed curb gutter line to match barrier gutter line. Referenced PV-2 for 4" curb detail.	
 APPROVED BY DESIGN METHODS ENGINEER	
DOUBLE REINFORCED 10" APPROACH WITH VARIABLE DEPTH PAVING NOTCH	



- ① 2" to 2½" clear to bent bar.
- ② Minimum lap length: #5 bars - 18 inches
#6 bars - 27 inches
#8 bars - 48 inches
- ③ If bridge is skewed, place additional #5 bar parallel to skewed face.

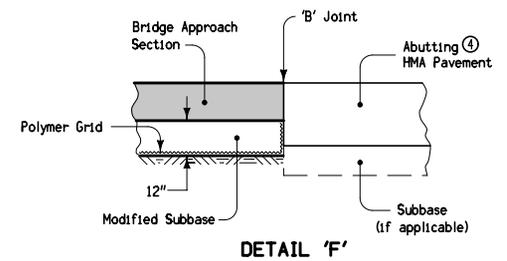
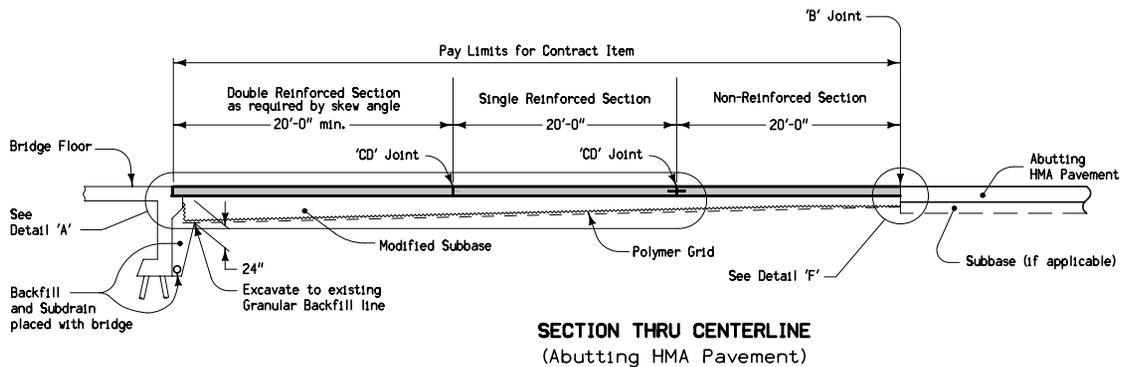
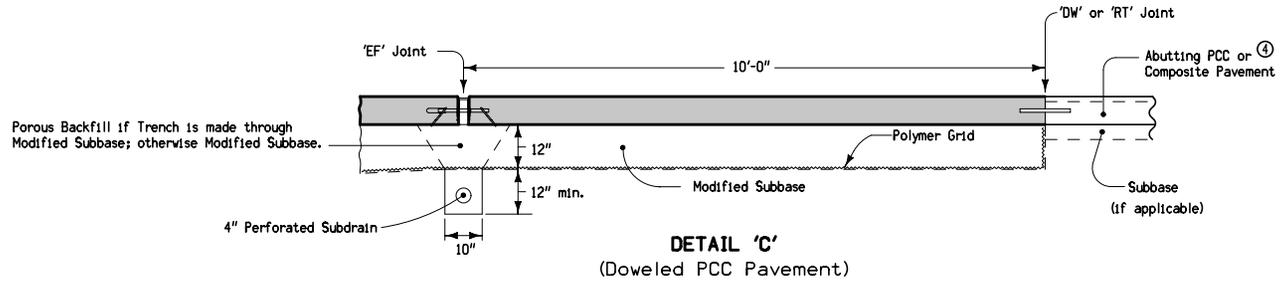
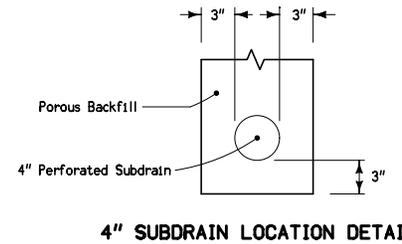
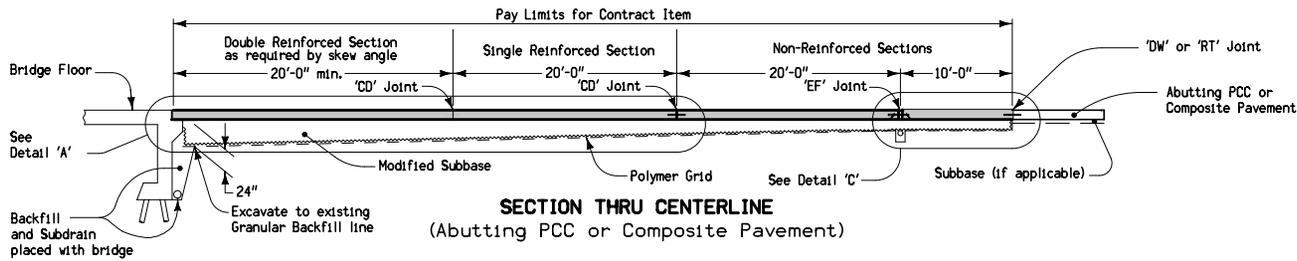


 Iowa Department of Transportation	REVISION	
	5	10-19-10
STANDARD ROAD PLAN	RK-26	
	SHEET 2 of 4	

REVISIONS: Changed curb gutter line to match barrier gutter line. Referenced PV-2 for 4" curb detail.

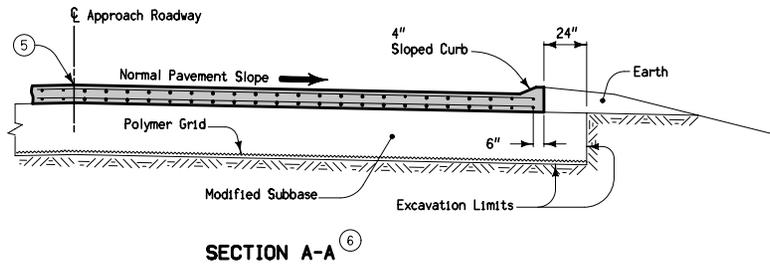
Deanna McFalls
APPROVED BY DESIGN METHODS ENGINEER

DOUBLE REINFORCED 10" APPROACH WITH VARIABLE DEPTH PAVING NOTCH

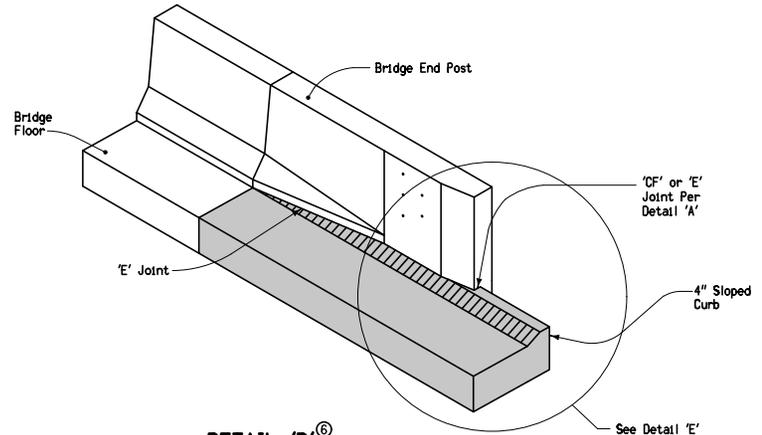


④ If abutting pavement (PCC or HMA) is not in place, see RK-30.

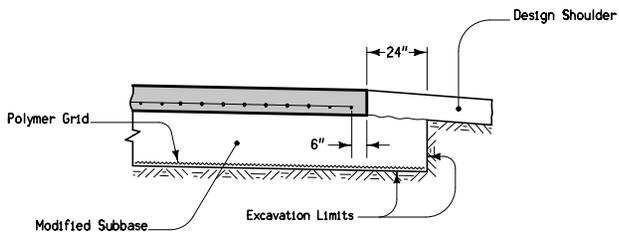
<p>Iowa Department of Transportation</p> <p>STANDARD ROAD PLAN</p>	REVISION	
	<table border="1"> <tr> <td>5</td> <td>10-19-10</td> </tr> </table>	5
5	10-19-10	
	RK-26	
	SHEET 3 of 4	
<small>REVISIONS: Changed curb gutter line to match barrier gutter line. Referenced PV-2 for 4" curb detail.</small>		
<p>APPROVED BY DESIGN METHODS ENGINEER</p>		
<p>DOUBLE REINFORCED 10" APPROACH WITH VARIABLE DEPTH PAVING NOTCH</p>		



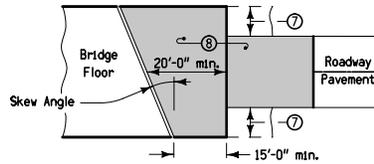
SECTION A-A ⑥



DETAIL 'D' ⑥
(Joint Placement)

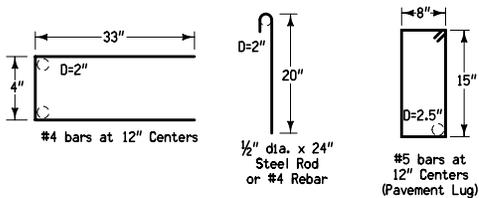


SECTION B-B ⑥

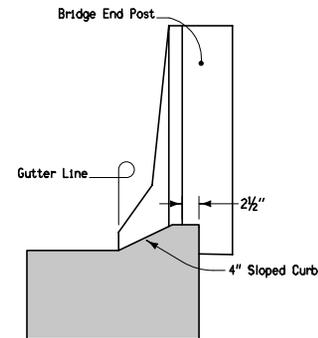


APPROACH PAVEMENT
LAYOUT AT A SKEW

- ⑤ Longitudinal Joint: (PV-1)
Single pour - Saw cut joint per Detail B
Two pours - use 'KS-2' Joint
- ⑥ See RK-21, RK-22, or RK-23.
- ⑦ Design shoulder width.
- ⑧ Reinforced bridge approach section.



BENT BAR SHAPES



DETAIL 'E'
(Back of Curb Placement)

 Iowa Department of Transportation STANDARD ROAD PLAN	REVISION	
	5	10-19-10
	RK-26	
SHEET 4 of 4		

REVISIONS: Changed curb gutter line to match barrier gutter line. Referenced PV-2 for 4" curb detail.

Deanna Macfild
APPROVED BY DESIGN METHODS ENGINEER

**DOUBLE REINFORCED 10" APPROACH
WITH VARIABLE DEPTH PAVING NOTCH**