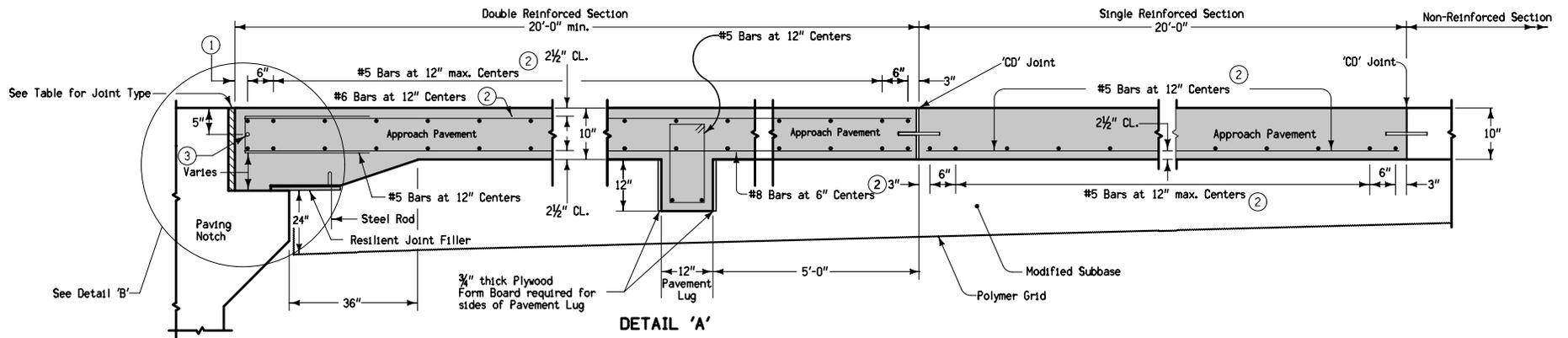
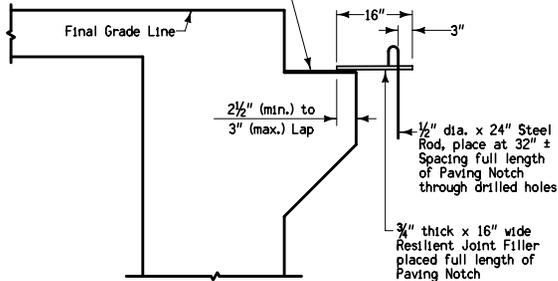


DETAIL 'A'
(Moveable Abutment)
(Section at 10" Deep Paving Notch)



DETAIL 'A'
(Moveable Abutment)
(Section Thru Centerline)

Debond paving notch with two (2) layers of 30# asphaltic felt paper full length of Paving Notch



DETAIL 'B'
(Moveable Abutment)

JOINT TYPE FOR MOVEABLE ABUTMENT BRIDGES

Joint	Maximum Bridge Length	
	Concrete Beam or Slab	Steel Girder
CF-1	370'	250'
CF-2	465'	320'
CF-3	575'	400'

For joint details, see PV-1.

All Transverse Bars are #5.

- ① 2" to 2½" clear to bent bar.
- ② Minimum lap length: #5 bars - 18 inches
#6 bars - 27 inches
#8 bars - 48 inches
- ③ If bridge is skewed, place additional #5 bar parallel to skewed face.

Possible Contract Item:

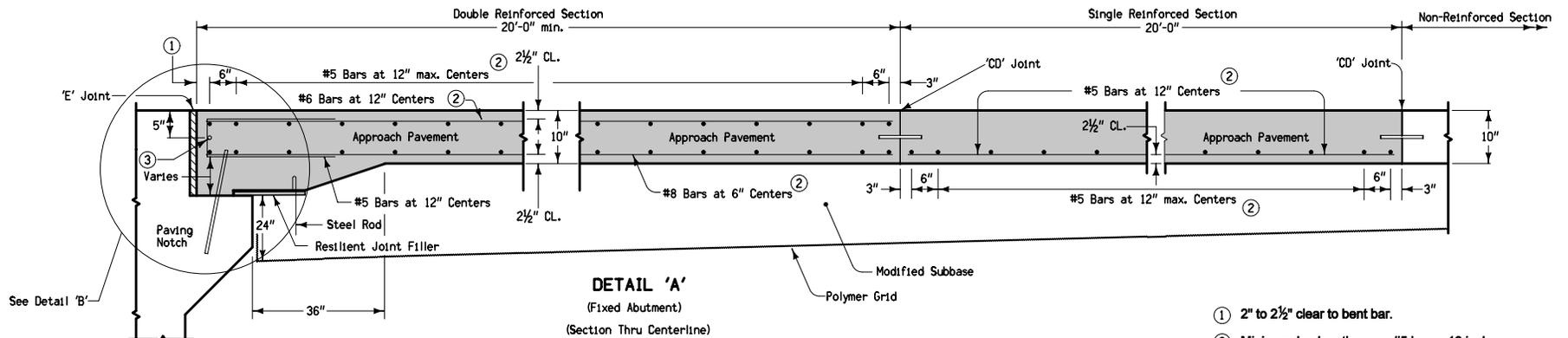
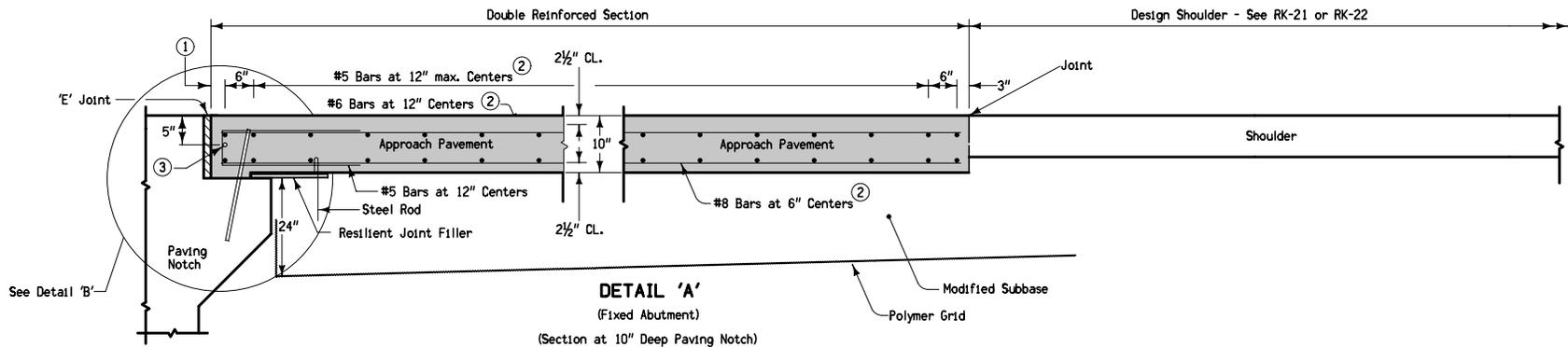
Bridge Approach, RK-26

Possible Tabulation:

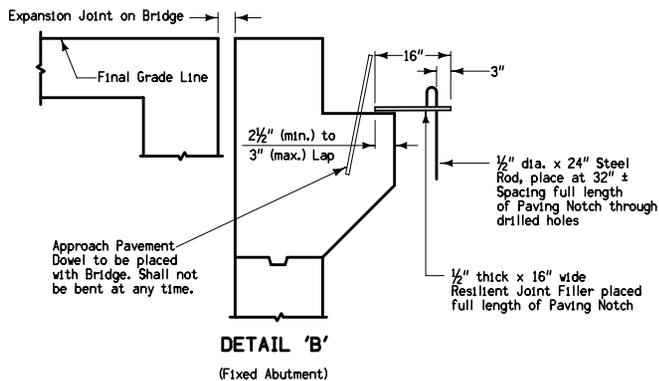
112-6

<p>Iowa Department of Transportation</p> <p>STANDARD ROAD PLAN</p> <p>REVISIONS: Updated references to renamed standards.</p> <p><i>Deanna McFalls</i> APPROVED BY DESIGN METHODS ENGINEER</p>	<p>REVISION</p> <p>4 04-20-10</p>
	<p>RK-26</p> <p>SHEET 1 of 4</p>

DOUBLE REINFORCED 10" APPROACH WITH VARIABLE DEPTH PAVING NOTCH



- ① 2" to 2½" clear to bent bar.
- ② Minimum lap length: #5 bars - 18 inches
#6 bars - 27 inches
#8 bars - 48 inches
- ③ If bridge is skewed, place additional #5 bar parallel to skewed face.

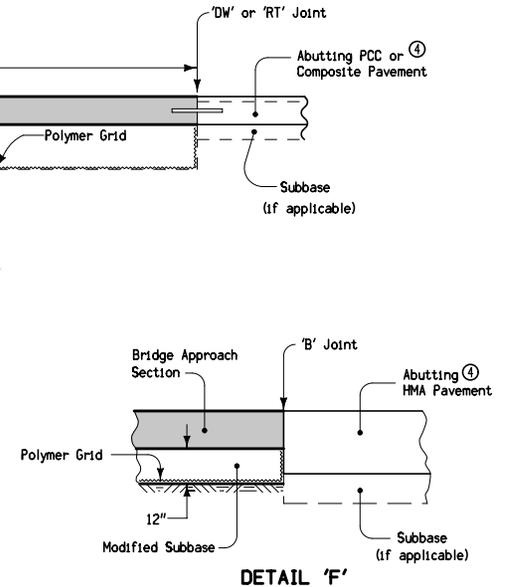
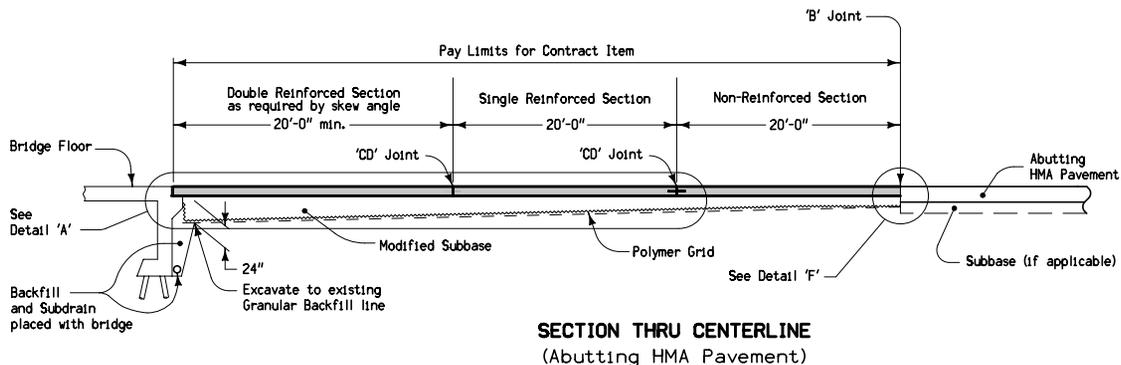
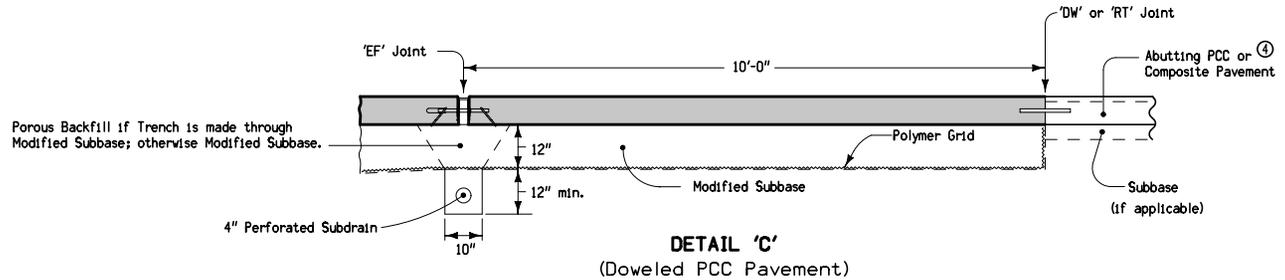
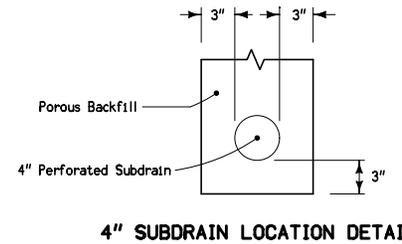
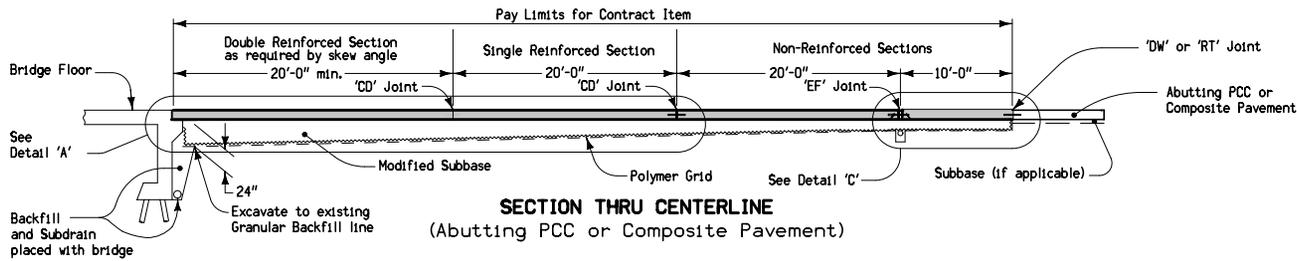


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REVISIONS: Updated references to renamed standards.

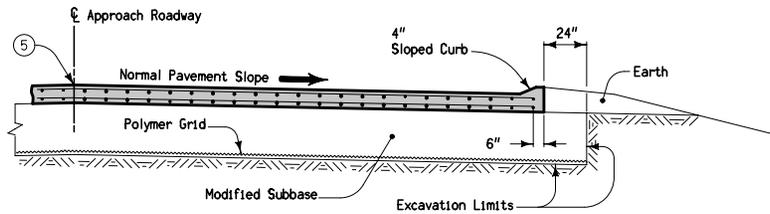
Deanna McFalls
APPROVED BY DESIGN METHODS ENGINEER

**DOUBLE REINFORCED 10" APPROACH
WITH VARIABLE DEPTH PAVING NOTCH**

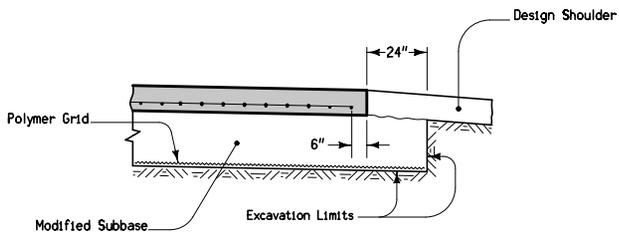


④ If abutting pavement (PCC or HMA) is not in place, see RK-30.

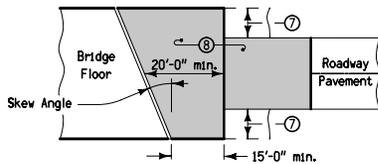
<p>Iowa Department of Transportation</p> <p>STANDARD ROAD PLAN</p> <p>REVISIONS: Updated references to renamed standards.</p> <p><i>Deanna McFalls</i> APPROVED BY DESIGN METHODS ENGINEER</p> <p>DOUBLE REINFORCED 10" APPROACH WITH VARIABLE DEPTH PAVING NOTCH</p>	<p>REVISION</p> <p>4 04-20-10</p>
	<p>RK-26</p> <p>SHEET 3 of 4</p>



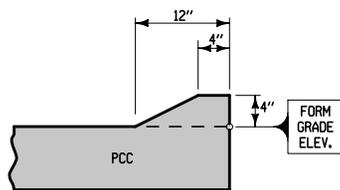
SECTION A-A (6)



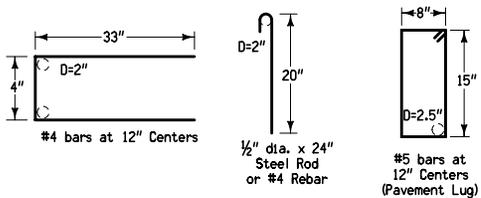
SECTION B-B (6)



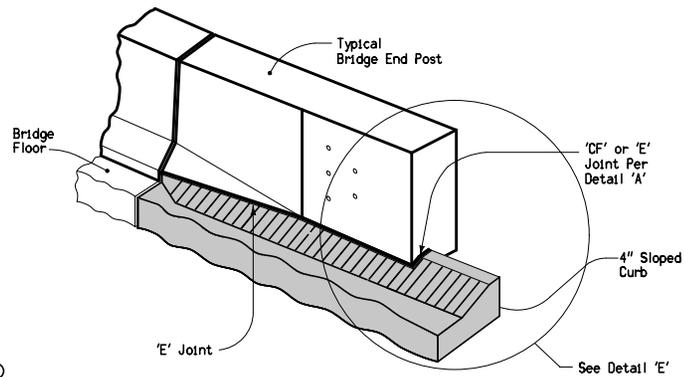
APPROACH PAVEMENT LAYOUT AT A SKEW



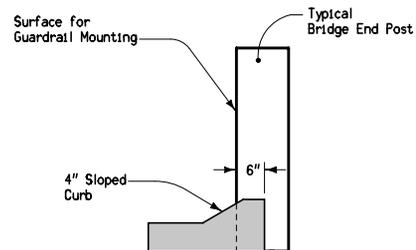
4" SLOPED CURB



BENT BAR SHAPES



DETAIL 'D' (Joint Placement)



DETAIL 'E' (Back of Curb Placement)

- (5) Longitudinal Joint: (PV-1)
Single pour - Saw cut joint per Detail B
Two pours - use 'KS-2' Joint
- (6) See RK-21, RK-22, or RK-23.
- (7) Design shoulder width.
- (8) Reinforced bridge approach section.

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DOUBLE REINFORCED 10" APPROACH WITH VARIABLE DEPTH PAVING NOTCH	