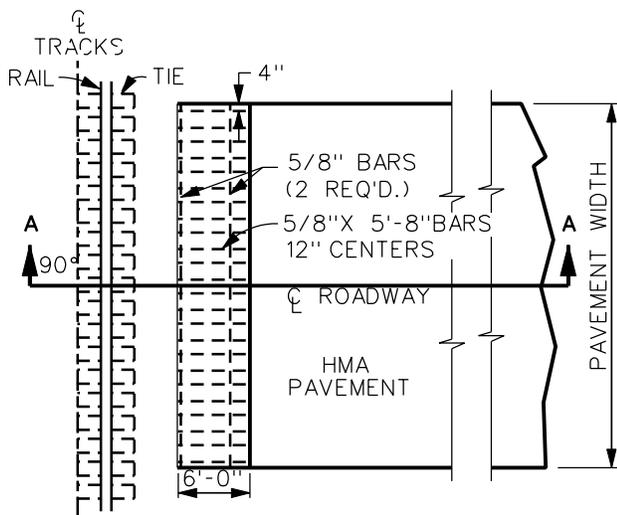
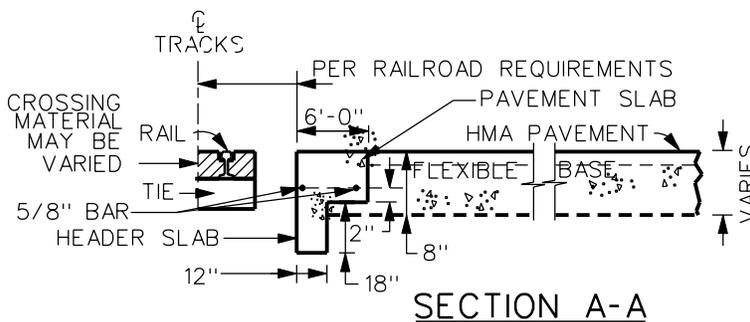


HALF PLAN
TYPICAL SKEWED CROSSING WITH FLEXIBLE PAVEMENT
(SYMMETRICAL ABOUT \bar{C} OF TRACKS)



HALF PLAN
TYPICAL STRAIGHT CROSSING
WITH FLEXIBLE PAVEMENT



SECTION A-A

NOTES:

1. DETAILS HEREON SHOW TYPICAL CONSTRUCTION FOR PAVEMENT HEADERS ADJACENT TO RAILROAD CROSSINGS.
2. CONCRETE FOR HEADER SHALL BE THE SAME AS FOR P.C.C. PAVEMENT.
3. HEADER SHALL BE FINISHED SMOOTH & SHALL HAVE BEEN PLACED AT LEAST 24 HOURS IN ADVANCE OF PAVEMENT.
4. THE EXCAVATION FOR THE HEADER SHALL BE MADE IN COMPACTED SUBGRADE TO THE MINIMUM NEAT LINES INDICATED HEREON.
5. SLOUGHED-OFF EARTH SHALL BE REMOVED & REPLACED WITH CONCRETE.
6. THE USE OF FORMS FOR SIDES OF THE HEADER WILL NOT BE ALLOWED.
7. FORMING OF THE ENDS WILL BE PERMITTED IF ANY SUCH FORMS ARE PLACED OUTSIDE THE LIMITS OF THE PAVEMENT SECTION.
8. CONCRETE FOR HEADER IS ESTIMATED AT 0.0556 CUBIC YARDS PER FOOT.
9. CONTACT RAILROAD FOR REQUIRED SET BACK DISTANCE FROM CENTER-LINE OF TRACKS TO HEADER AND EDGE OF PAVING.
10. CONCRETE HEADER AND PAVING MAY BE OMITTED UPON APPROVAL OF BOTH THE RAILROAD AND THE JURISDICTIONAL ENGINEER.
11. SPACE BETWEEN CROSSING AND P.C.C. PAVING SHALL BE FILLED WITH HMA IN ACCORDANCE WITH SECTION 7020.

2	10/19/04	
REV.	DATE	BY
DATE: 01-01-98		

**RAILROAD CROSSING
(HMA)**

FIGURE: 7020.9
SHEET 1 OF 1