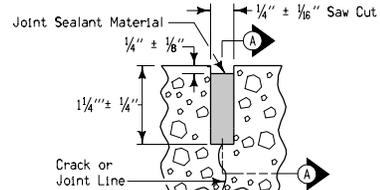
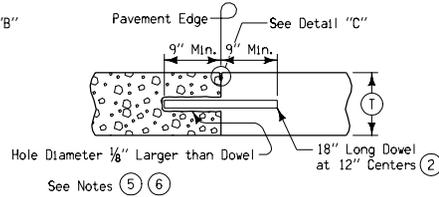
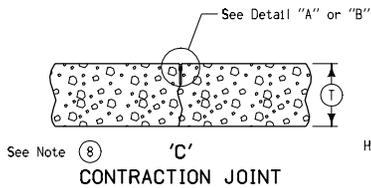
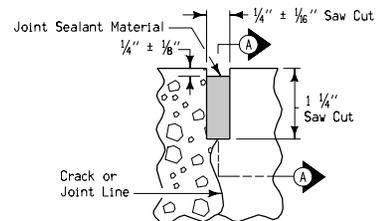
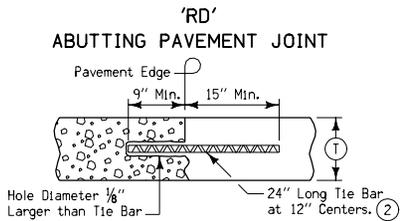
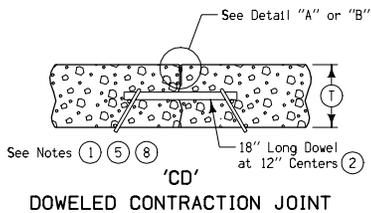


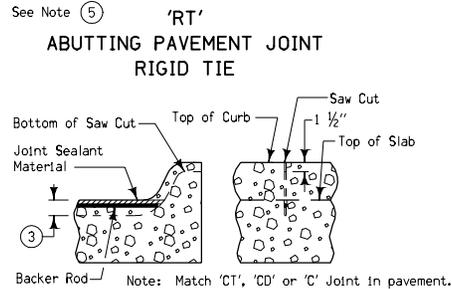
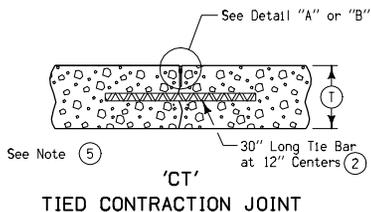
DETAIL "A"
(Sawcut formed by conventional concrete saving equipment)



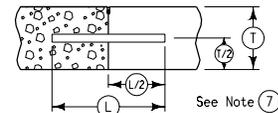
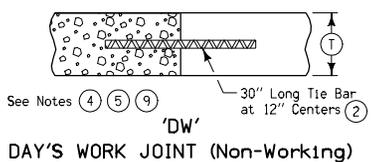
DETAIL "B"
(Sawcut formed by approved early concrete saving equipment)



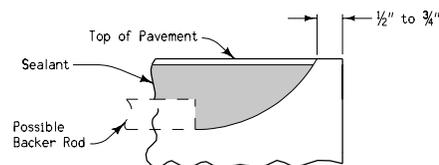
DETAIL "C"



'C' JOINT IN CURB



TYPICAL BAR PLACEMENT
Applies to all joints unless otherwise detailed.



SECTION A-A
DETAIL AT EDGE OF PAVEMENT

- ① See Standard Road Plan RH-55 for dowel assembly fabrication details.
- ② Refer to Bar Size Table.
- ③ Depth of sawcut shall be T/3, except 'C' joint shall be T/4.
- ④ 'DW' joint shall be located at a midpanel location between future 'C' or 'CD' joints. It shall be no closer than 5'-0" to a 'C' or 'CD' joint.
- ⑤ Bars shall be placed within the limits shown on Standard Road Plan RH-55.
- ⑥ Edge with 1/4" tool for length of joint indicated if formed; edging not required when cut with diamond blade saw. Remove header block and board when second slab is poured.
- ⑦ When tying into old pavement, T represents the depth of sound Portland Cement Concrete.
- ⑧ Unless otherwise specified, transverse contraction joints in mainline pavement shall be 'CD' when T is greater or equal to 8'. 'C' when T is less than 8'.
- ⑨ 'RT' joint may be used in lieu of 'DW' joint at the end of the days work. Any pavement damaged due to the drilling shall be removed at the contractor's expense.

BAR SIZE TABLE			
T	< 8"	≥ 8" but < 10"	≥ 10"
Dowel Diameter	3/4"	1 1/4"	1 1/2"
Tie Bar Size	#6	#10	#11

Iowa Department of Transportation
Highway Division

STANDARD ROAD PLAN RH-50

REVISION: Update references in notes.	REVISION NO. 15
<i>Deanna Maxwell</i>	REVISION DATE 04-18-06
APPROVED BY DESIGN METHODS ENGINEER	

JOINTS
(TRANSVERSE CONTRACTION)