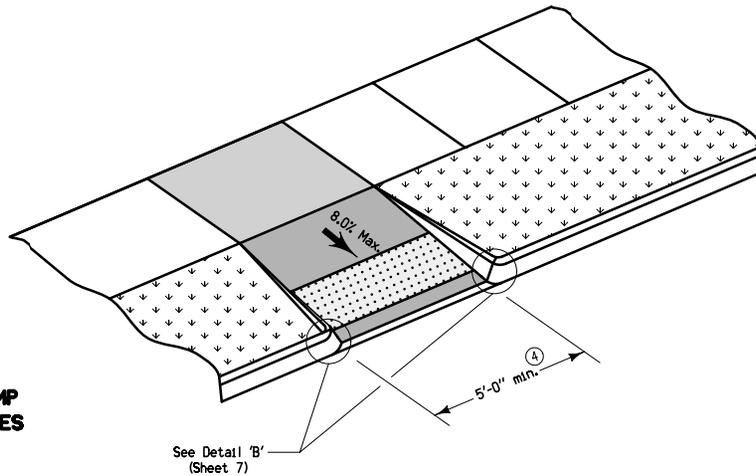
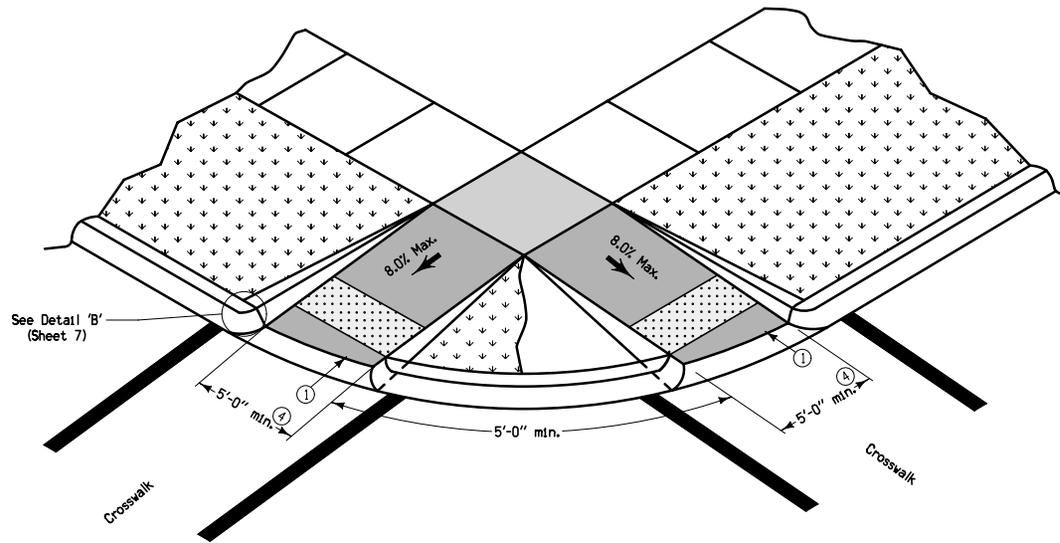


**TYPE 1-R  
PERPENDICULAR RAMP  
WITH RETURNED SIDES**



- ① Unless curb ramp is aligned perpendicular to the street radius, an area of special shaping must be provided at the bottom of the ramp. This area shall allow the grade break at the bottom of the ramp to be perpendicular to the ramp and shall provide a smooth transition to gutterline for wheelchair access. No curb lip allowed in this area. Maximum cross slope shall be 2%.
- ② Maximum cross slope on all ramps is 2%.
- ③ A 5' min. x 5' min. landing area shall be provided at all curb ramps. Maximum slope in any direction is 2%.
- ④ Distance measured from face of curb to face of curb.



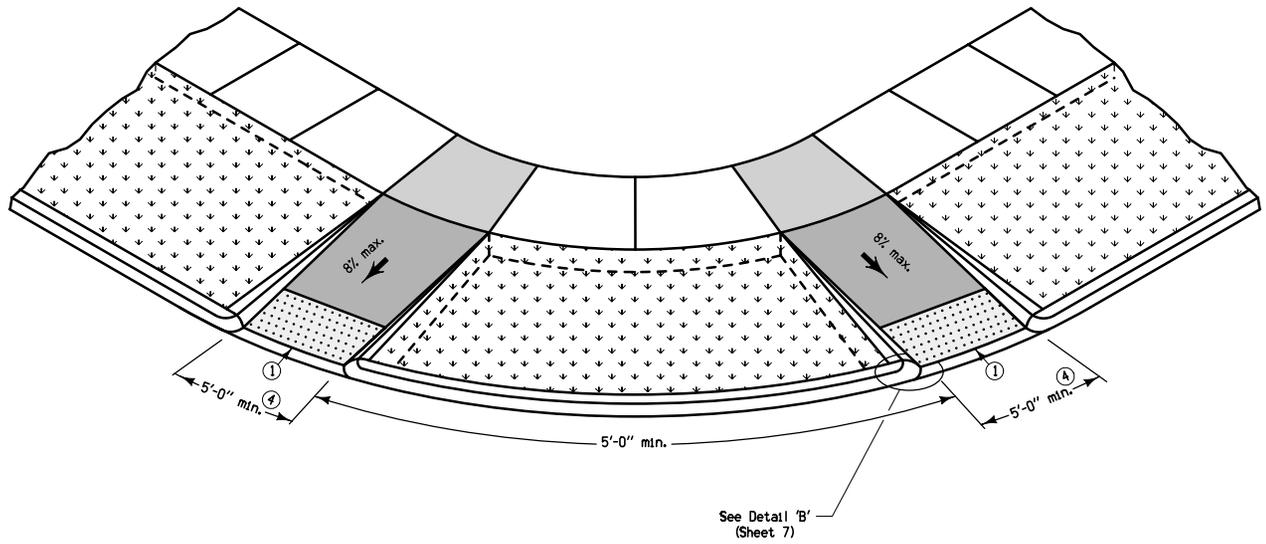
**LEGEND**

	Curb Ramp ②
	Landing Area ③
	Detectable Warnings
	Vegetation

Possible Contract Items:  
PCC Sidewalk, 6 inch  
Detectable Warnings  
Possible Tabulation: 112-4

**TYPICAL APPLICATION  
OF TYPE 1-R RAMP  
AT A CORNER**

<p>Iowa Department of Transportation</p>	REVISION
	15   10-20-09
<b>STANDARD ROAD PLAN</b>	<b>RB-6</b>
SHEET 1 of 7	
<small>REVISIONS: Added detail for skewed railroad crossings. Changed ramp width to 5 ft. DOT standard and circle note 3.</small>	
<i>Deanna McFalls</i> <small>APPROVED BY DESIGN METHODS ENGINEER</small>	
<b>PEDESTRIAN CURB RAMPS</b>	



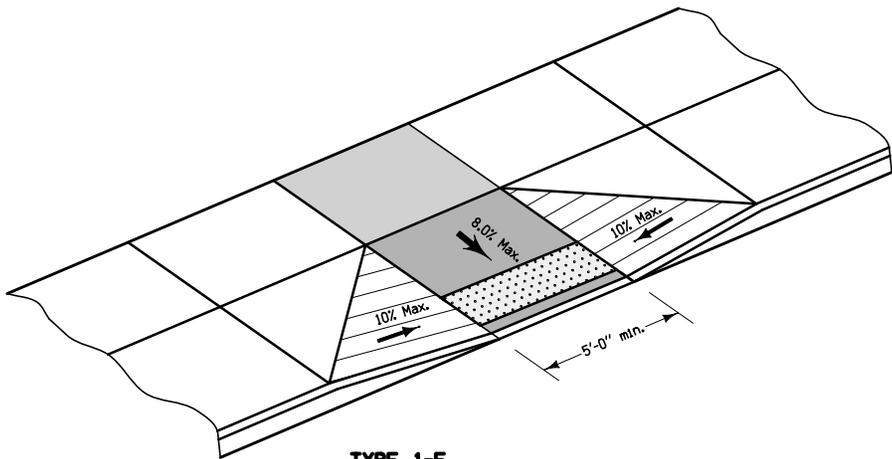
- ① Unless curb ramp is aligned perpendicular to the street radius, an area of special shaping must be provided at the bottom of the ramp. This area shall allow the grade break at the bottom of the ramp to be perpendicular to the ramp and shall provide a smooth transition to gutterline for wheelchair access. No curb lip allowed in this area. Maximum cross slope shall be 2%.
- ② Maximum cross slope on all ramps is 2%.
- ③ A 5' min. x 5' min. landing area shall be provided at all curb ramps. Maximum slope in any direction is 2%.
- ④ Distance measured from face of curb to face of curb.

**LEGEND**

	Curb Ramp ②
	Landing Area ③
	Detectable Warnings
	Vegetation

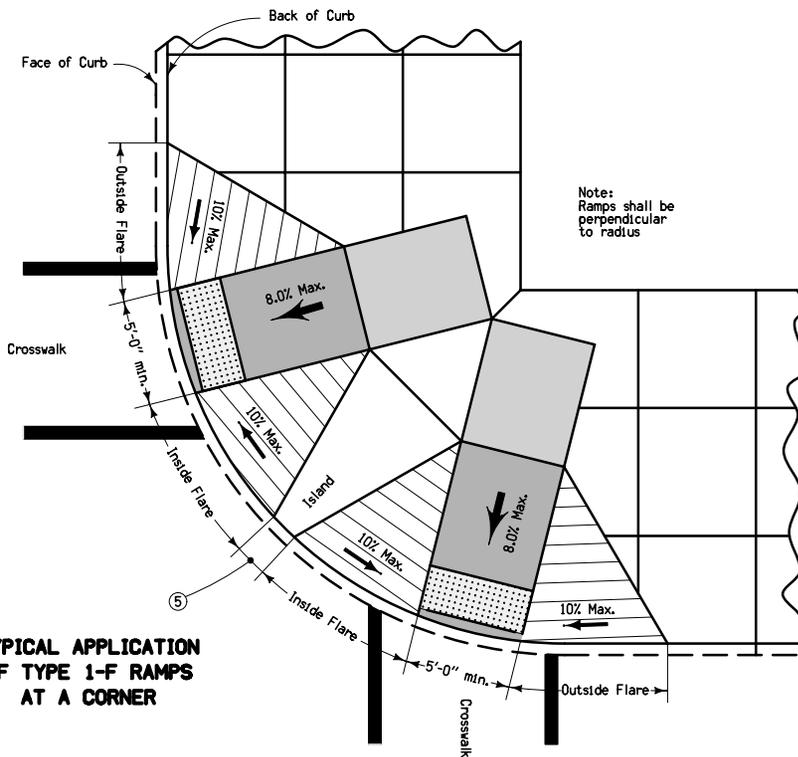
**TYPICAL LARGE RADIUS APPLICATION OF TYPE 1-R**

<p>Iowa Department of Transportation</p> <p><b>STANDARD ROAD PLAN</b></p> <p>REVISIONS: Added detail for skewed railroad crossings. Changed ramp width to 5 ft. DOT standard and circle note 3.</p> <p><i>Deanna Mifflin</i> APPROVED BY DESIGN METHODS ENGINEER</p>	<p>REVISION</p> <table border="1"> <tr> <td>15</td> <td>10-20-09</td> </tr> </table>	15	10-20-09
	15	10-20-09	
	<p><b>RB-6</b></p> <p>SHEET 2 of 7</p>		
<p><b>PEDESTRIAN CURB RAMPS</b></p>			



**TYPE 1-F  
PERPENDICULAR RAMP  
WITH FLARED SIDES**

- ② Maximum cross slope on all ramps is 2%.
- ③ A 5' min. x 5' min. landing area shall be provided at all curb ramps. Maximum slope in any direction in 2%.
- ⑤ Width varies; if center island cannot be constructed at a minimum width of 2' and height of 3" by reducing the width of the island flares, refer to Type 5.



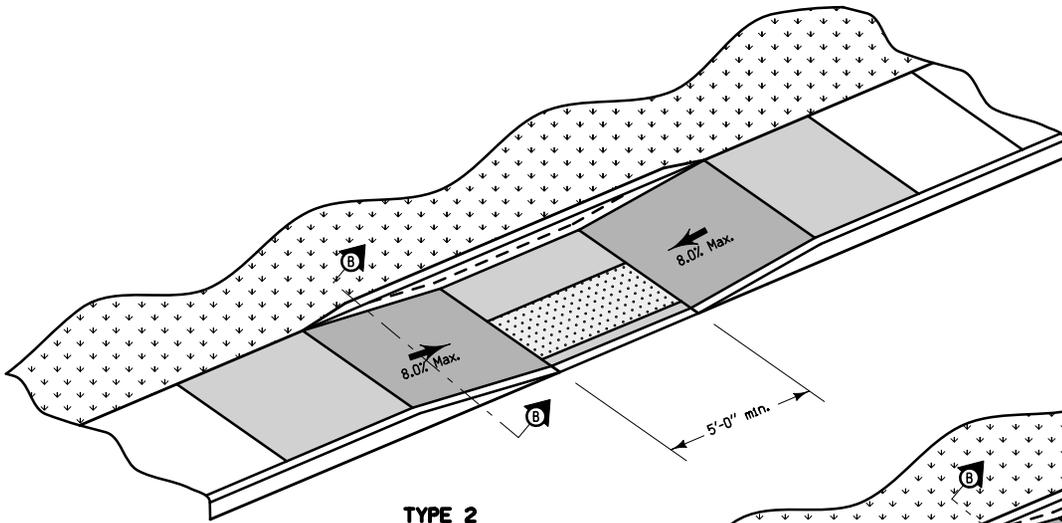
**TYPICAL APPLICATION  
OF TYPE 1-F RAMPS  
AT A CORNER**

**LEGEND**

	Curb Ramp ②
	Landing Area ③
	Detectable Warnings
	Flared Sides

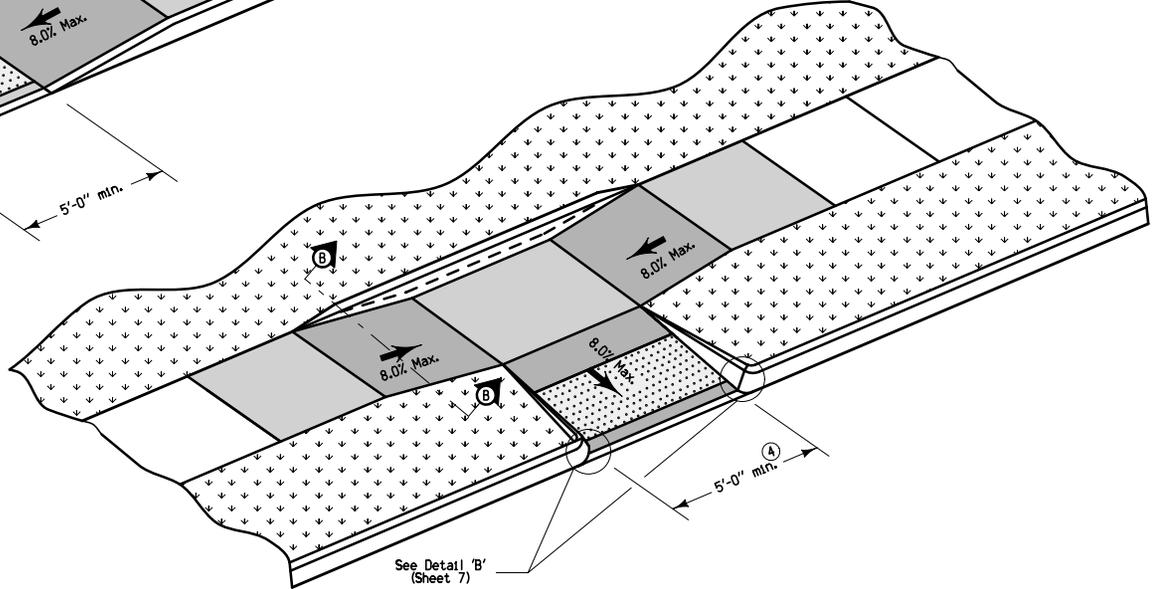
 Iowa Department of Transportation	REVISION
	15   10-20-09
	<b>RB-6</b>
	SHEET 3 of 7
REVISIONS: Added detail for skewed railroad crossings. Changed ramp width to 5 ft. DOT standard and circle note 3.	
<i>Deanna Maifield</i> APPROVED BY DESIGN METHODS ENGINEER	
<b>PEDESTRIAN CURB RAMPS</b>	

- ② Maximum cross slope on all ramps is 2%.
- ③ A 5' min. x 5' min. landing area shall be provided at all curb ramps. Maximum slope in any direction is 2%.
- ④ Distance measured from face of curb to face of curb.



**TYPE 2  
PARALLEL RAMP**

(For use with restricted right-of-way)

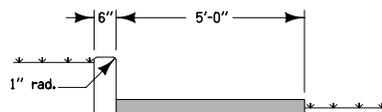


**TYPE 3  
COMBINATION RAMP**

(For use with restricted right-of-way)

**LEGEND**

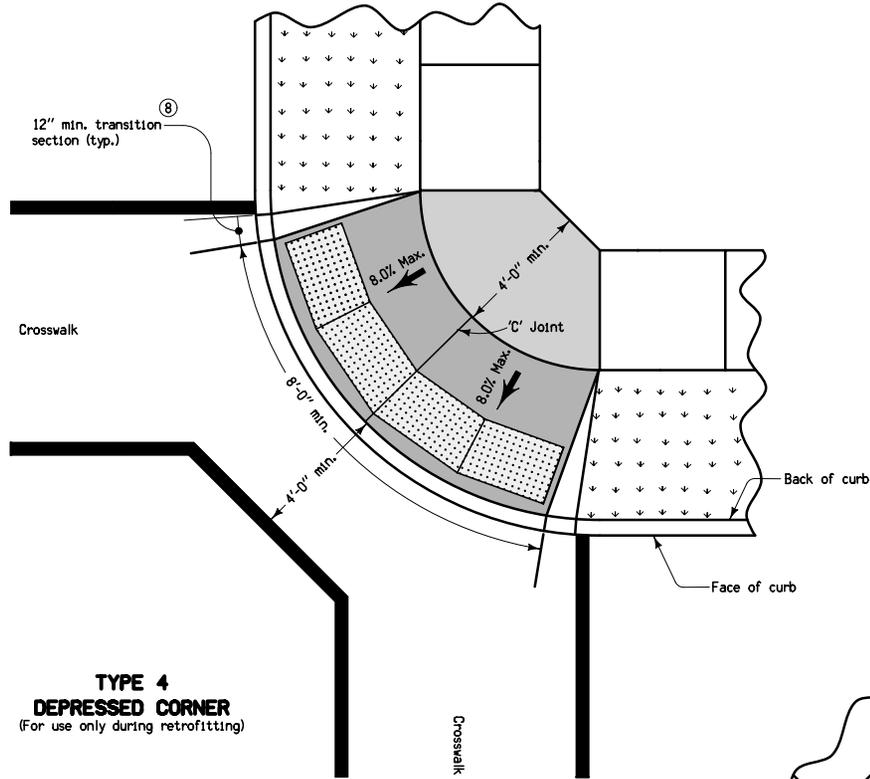
	Curb Ramp ②
	Landing Area ③
	Detectable Warnings
	Vegetation



**SECTION B-B**

<p>Iowa Department of Transportation</p>	REVISION
	15   10-20-09
<b>STANDARD ROAD PLAN</b>	<b>RB-6</b>
	SHEET 4 of 7
<small>REVISIONS: Added detail for skewed railroad crossings. Changed ramp width to 5 ft. DOT standard and circle note 3.</small>	
<i>Deanna Muford</i> <small>APPROVED BY DESIGN METHODS ENGINEER</small>	
<b>PEDESTRIAN CURB RAMPS</b>	

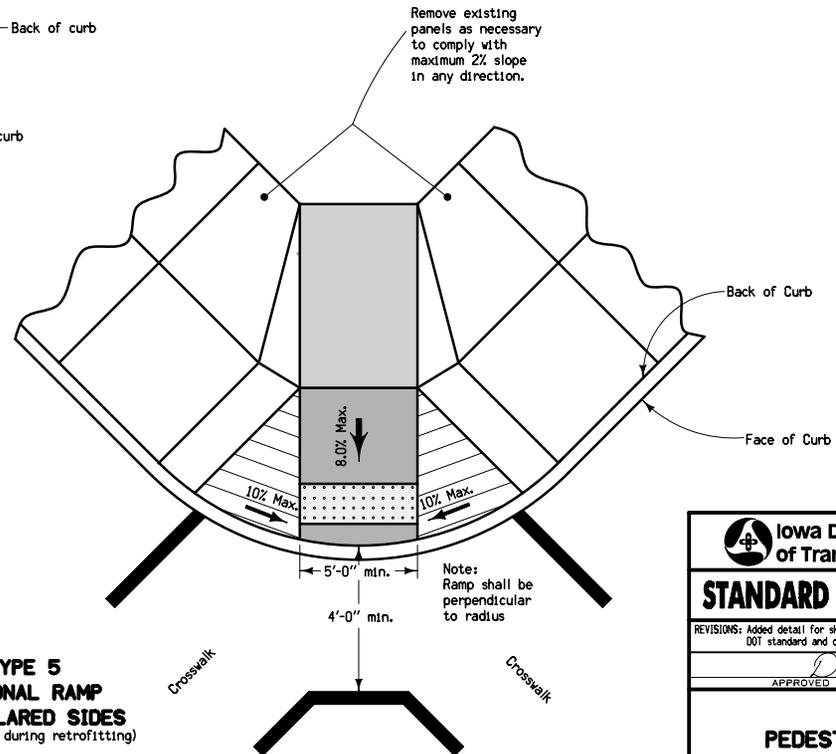
- ② Maximum cross slope on all ramps is 2%.
- ③ A 5' min. x 5' min. landing area shall be provided at all curb ramps. Maximum slope in any direction is 2%.
- ⑧ At existing curb and gutter locations, remove curb and build transition section.



**TYPE 4  
DEPRESSED CORNER**  
(For use only during retrofitting)

**LEGEND**

	Curb Ramp ②
	Landing Area ③
	Detectable Warnings
	Vegetation
	Flared Sides



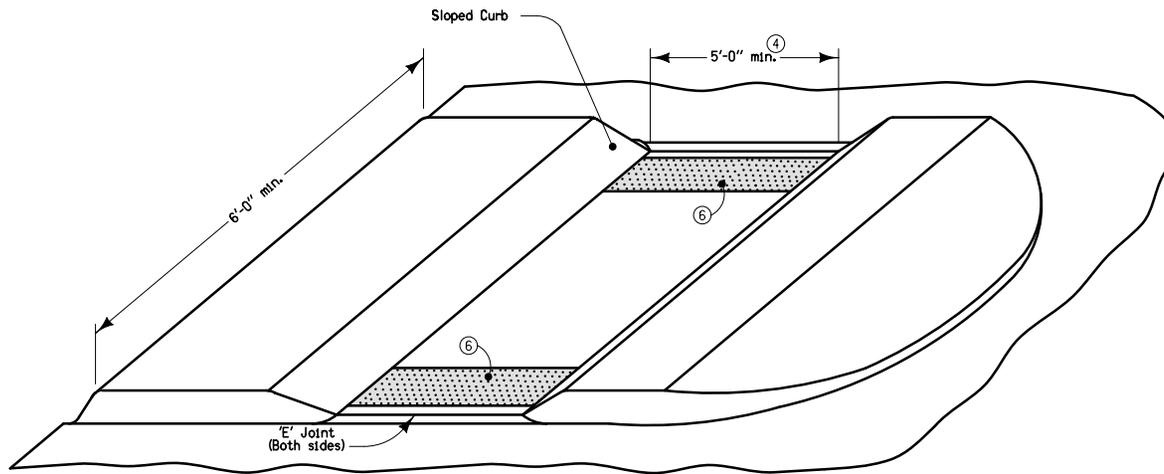
**TYPE 5  
DIAGONAL RAMP  
WITH FLARED SIDES**  
(For use only during retrofitting)

Remove existing panels as necessary to comply with maximum 2% slope in any direction.

Back of Curb  
Face of Curb

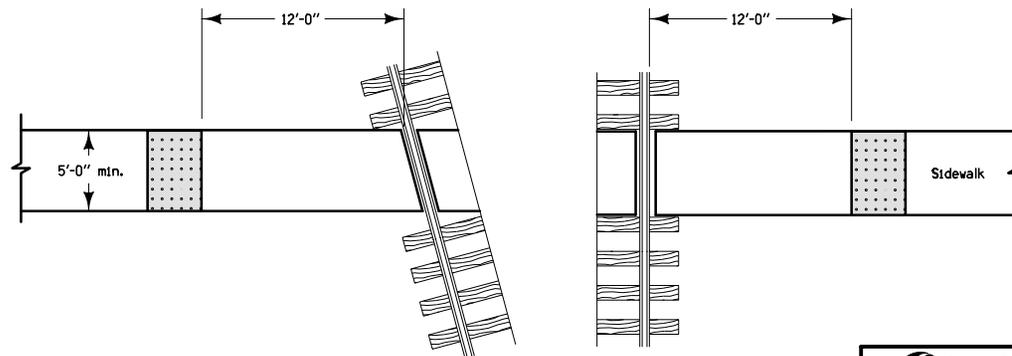
Note: Ramp shall be perpendicular to radius

<p>Iowa Department of Transportation</p> <p><b>STANDARD ROAD PLAN</b></p> <p>REVISIONS: Added detail for skewed railroad crossings. Changed ramp width to 5 ft. DOT standard and circle note 3.</p> <p><i>Deanna Mifflin</i> APPROVED BY DESIGN METHODS ENGINEER</p>	<p>REVISION</p> <table border="1"> <tr> <td>15</td> <td>10-20-09</td> </tr> </table>	15	10-20-09
	15	10-20-09	
	<p><b>RB-6</b></p> <p>SHEET 5 of 7</p>		
<p><b>PEDESTRIAN CURB RAMPS</b></p>			



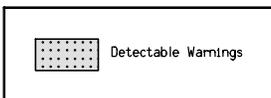
- ④ Distance measured from face of curb to face of curb.
- ⑥ For medians at signal-controlled crossing:  
Where signal timing is such that most pedestrians are likely to cross the entire roadway within one crossing phase, detectable warnings may be omitted.

**TYPE 6  
ACCESS AT CURBED MEDIANS**

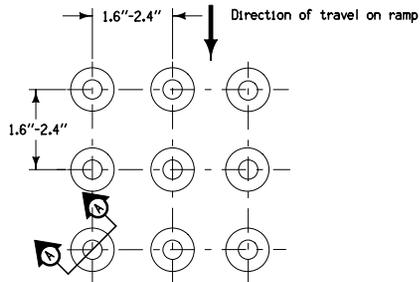


**TYPE 7  
RAILROAD CROSSING**

**LEGEND**

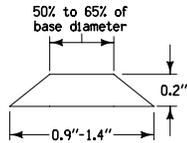


 Iowa Department of Transportation	REVISION	
	15	10-20-09
<b>STANDARD ROAD PLAN</b>		
<b>RB-6</b>		
SHEET 6 of 7		
<small>REVISIONS: Added detail for skewed railroad crossings. Changed ramp width to 5 ft. DOT standard and circle note 3.</small>		
<i>Deanna Mufitt</i> <small>APPROVED BY DESIGN METHODS ENGINEER</small>		
<b>PEDESTRIAN CURB RAMPS</b>		

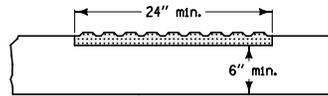


**SQUARE PATTERN**  
(PARALLEL ALIGNMENT)

**DETECTABLE WARNINGS**

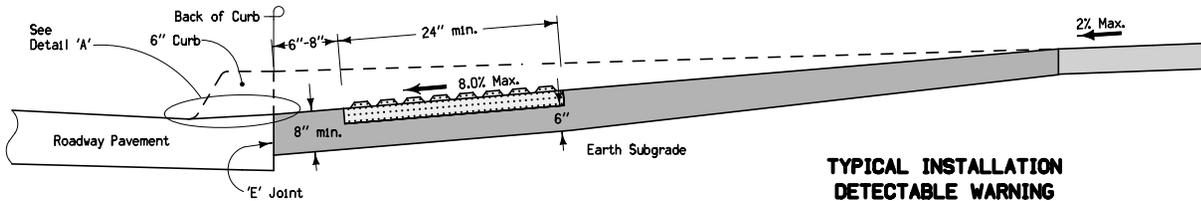


**SECTION A-A**

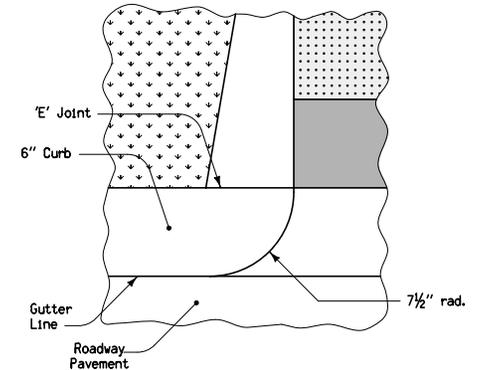


**DETECTABLE WARNING  
FLAT SURFACE INSTALLATION**

- ② Maximum cross slope on all ramps is 2%.
- ③ A 5' min. x 5' min. landing area shall be provided at all curb ramps. Maximum slope in any direction is 2%.
- ⑦ At points where a change in slope occurs, the algebraic grade difference shall not exceed 11%.



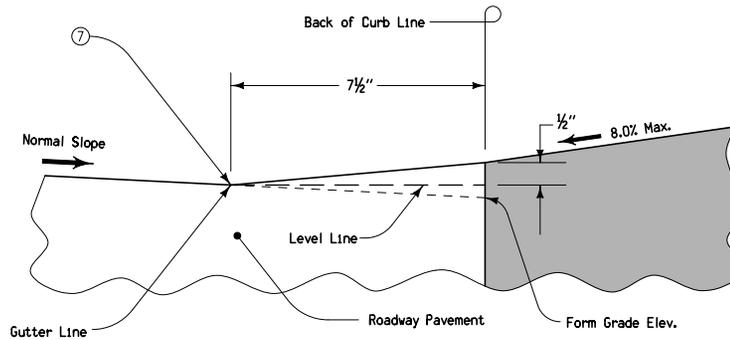
**TYPICAL INSTALLATION  
DETECTABLE WARNING**  
(ALL RAMP TYPES EXCEPT 2, 6, AND 7)



**DETAIL 'B'**

**LEGEND**

	Curb Ramp ②
	Landing Area ③
	Detectable Warnings
	Vegetation



**DETAIL 'A'**

<p>Iowa Department of Transportation</p> <p><b>STANDARD ROAD PLAN</b></p> <p>REVISIONS: Added detail for skewed railroad crossings. Changed ramp width to 5 ft. DOT standard and circle note 3.</p> <p><i>Deanna McFalls</i> APPROVED BY DESIGN METHODS ENGINEER</p>	<p>REVISION</p> <table border="1"> <tr> <td>15</td> <td>10-20-09</td> </tr> </table>	15	10-20-09
	15	10-20-09	
	<p><b>RB-6</b></p> <p>SHEET 7 of 7</p>		
	<p><b>PEDESTRIAN CURB RAMPS</b></p>		