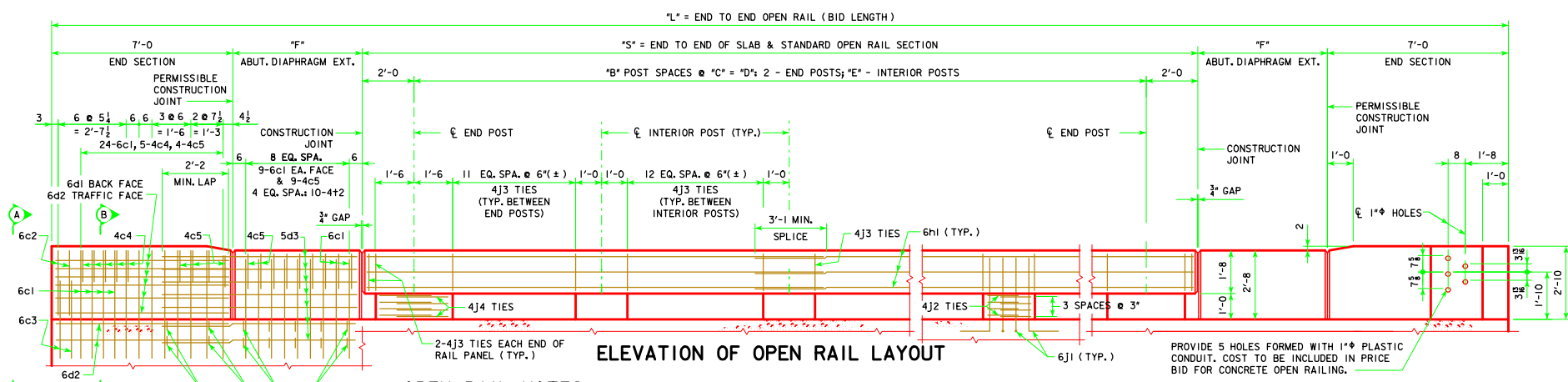
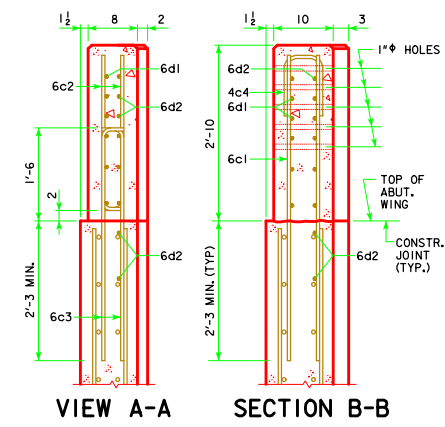


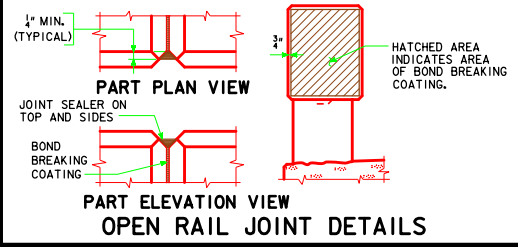
TABLE OF OPEN RAIL DIMENSIONS AND NUMBERS


E-E ABUT. BRG.		46'-8"			55'-0"			67'-6"			80'-0"			90'-0"			100'-0"			110'-0"			E-E ABUT. BRG.	
SKEW		0°	15°	30°	0°	15°	30°	0°	15°	30°	0°	15°	30°	0°	15°	30°	0°	15°	30°	0°	15°	30°	SKEW	
DIMENSION OR NUMBER	L (FT.-IN.)	63'-8"	63'-9 1/4"	64'-1 3/8"	72'-0"	72'-1 1/4"	72'-5 1/8"	84'-6"	84'-7 1/4"	84'-11 1/8"	107'-0"	107'-1 1/4"	107'-5 1/8"	117'-0"	117'-1 1/4"	117'-5 1/8"	127'-0"	127'-1 1/4"	127'-5 1/8"	137'-0"	137'-1 1/4"	137'-5 1/8"	L (FT.-IN.)	DIMENSION OR NUMBER
	S (FT.-IN.)	49'-8"	49'-9 1/4"	50'-1 3/8"	58'-0"	58'-1 1/4"	58'-5 1/8"	70'-6"	70'-7 1/4"	70'-11 1/8"	83'-0"	83'-1 1/4"	83'-5 1/8"	93'-0"	93'-1 1/4"	93'-5 1/8"	103'-0"	103'-1 1/4"	103'-5 1/8"	113'-0"	113'-1 1/4"	113'-5 1/8"	S (FT.-IN.)	
	B	6	6	6	7	7	7	9	9	9	10	10	10	12	12	12	13	13	13	14	14	14	B	
	C (FT.-IN.)	7'-7 1/2"	7'-7 1/2"	7'-8 1/4"	7'-8 1/4"	7'-8 1/4"	7'-9 1/8"	7'-4 3/4"	7'-4 3/4"	7'-5 1/4"	7'-10 3/4"	7'-10 3/4"	7'-11 1/8"	7'-5"	7'-5 1/4"	7'-5 1/2"	7'-7 3/8"	7'-7 1/2"	7'-7 3/4"	7'-9 3/8"	7'-9 1/2"	7'-9 3/4"	C (FT.-IN.)	
	D (FT.-IN.)	45'-8"	45'-9 1/4"	46'-1 3/8"	54'-0"	54'-1 1/4"	54'-5 1/8"	66'-6"	66'-7 1/4"	66'-11 1/8"	79'-0"	79'-1 1/4"	79'-5 1/8"	89'-0"	89'-1 1/4"	89'-5 1/8"	99'-0"	99'-1 1/4"	99'-5 1/8"	109'-0"	109'-1 1/4"	109'-5 1/8"	D (FT.-IN.)	
	E	5	5	5	6	6	6	8	8	8	9	9	9	11	11	11	12	12	12	13	13	13	E	
F (FT.-IN.)	0	0	0	0	0	0	0	0	0	0	5'-0"	5'-0"	5'-0"	5'-0"	5'-0"	5'-0"	5'-0"	5'-0"	5'-0"	5'-0"	5'-0"	F (FT.-IN.)		



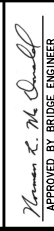
OPEN RAIL NOTES:

- CONSTRUCTION JOINT BETWEEN TOP OF WING AND RAIL IS ROUGHENED CONCRETE.
- MINIMUM CLEAR DISTANCE FROM FACE OF CONCRETE TO NEAR REINFORCING BAR IS TO BE 2" UNLESS OTHERWISE NOTED OR SHOWN.
- COST OF THE JOINT SEALER AND BOND BREAKER SHALL BE CONSIDERED INCIDENTAL TO OTHER CONSTRUCTION.
- ALL OPEN RAIL REINFORCING STEEL IS TO BE INCLUDED WITH THE SUPERSTRUCTURE REINFORCING STEEL.
- THE CAST-IN-PLACE OPEN RAIL SHALL USE CLASS C MIX. CLASS D CONCRETE IS NOT PERMITTED.
- TOP OF THE OPEN RAIL IS TO BE PARALLEL TO THEORETICAL E GRADE.




Iowa Department of Transportation
Highway Division

STANDARD DESIGN - 30' ROADWAY, SINGLE SPAN BRIDGE
PRETENSIONED PRESTRESSED CONCRETE BEAM BRIDGES
 APRIL, 2012

LATEST REVISION DATE APPROVED BY BRIDGE ENGINEER 	OPEN RAIL, TL-4 DETAILS H30SI-34-12 SHEET 1 OF 2
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