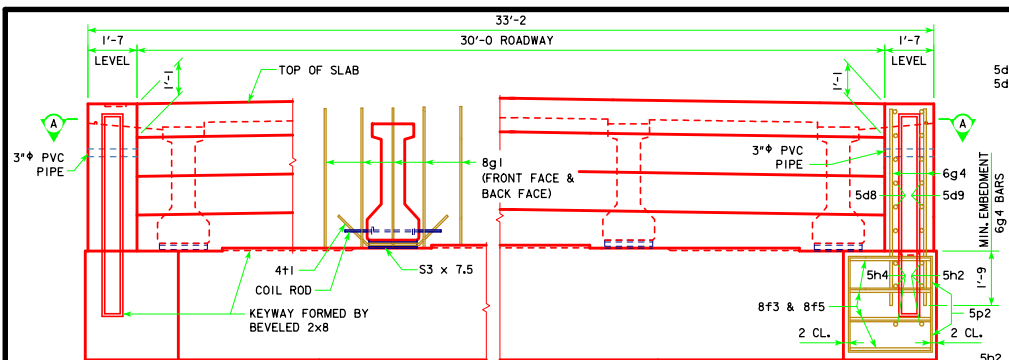
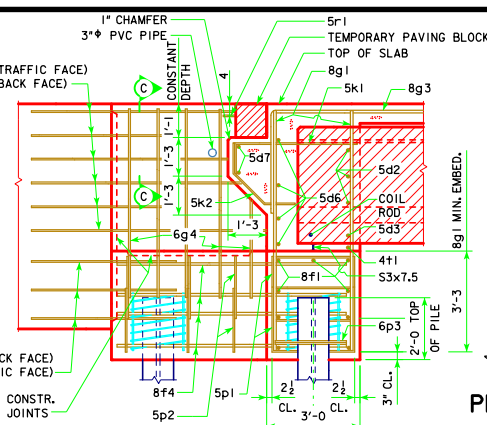


REVISED 04-13 - REVISION FOR LRPD PILE DESIGN.



PART REAR ELEVATION AT ABUTMENT
NOTE: TOP OF ABUTMENT SHOWN FOR SOLID BARRIER RAIL



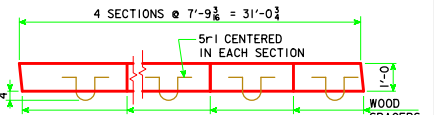
PART SECTION B-B

ABUTMENT NOTES:
MINIMUM CLEAR DISTANCE FROM FACE OF CONCRETE TO NEAR REINFORCING BAR IS TO BE 2" UNLESS OTHERWISE NOTED OR SHOWN.

ABUTMENT PILES SHALL BE DRIVEN TO VALUES SHOWN IN DESIGN PLANS.

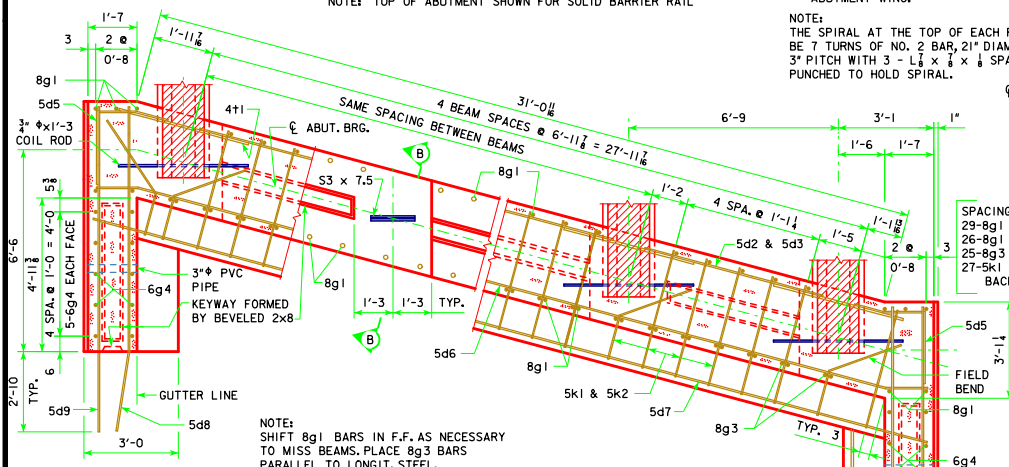
BARRIER RAIL NOT SHOWN IN DETAILS.

IF ROCK IS CLOSER THAN 15' BELOW ABUTMENT FOOTING, SPECIAL ANALYSIS MAY BE REQUIRED.



PLAN OF TEMPORARY PAVING BLOCK

NOTE: LINE PAVING NOTCH WITH TAR PAPER BEFORE PLACING THE TEMPORARY PAVING BLOCK.

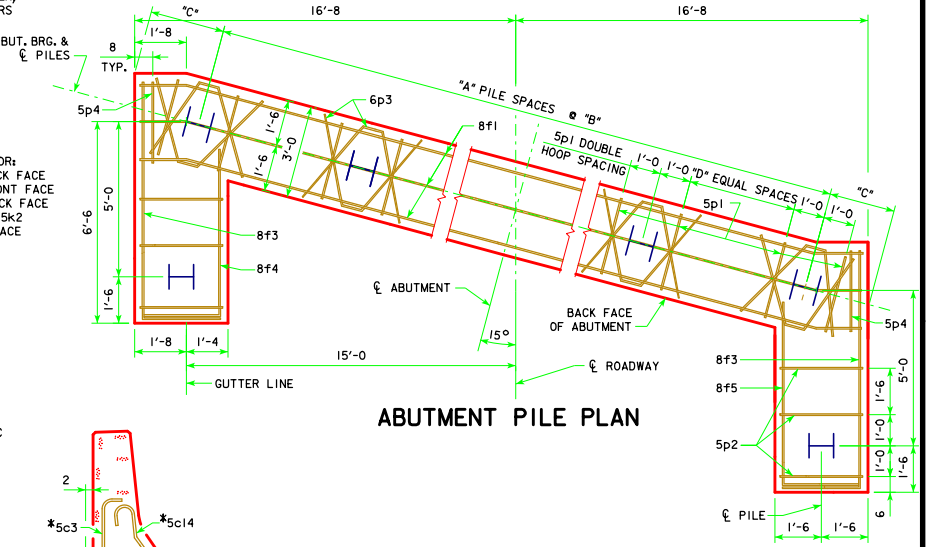


PART SECTION A-A

NOTE: SHIFT 8g1 BARS IN F.F. AS NECESSARY TO MISS BEAMS. PLACE 8g3 BARS PARALLEL TO LONGIT. STEEL.

NOTE: THE SPIRAL AT THE TOP OF EACH PILE TO BE 7 TURNS OF NO. 2 BAR, 21\"/>

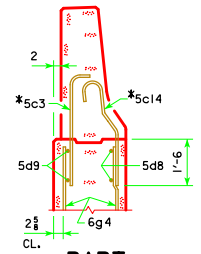
SPACING FOR:
29-8g1 BACK FACE
26-8g1 FRONT FACE
25-8g3 BACK FACE
27-5k1 & 5k2 BACK FACE



ABUTMENT PILE PLAN

ABUTMENT PILE SPACING		CL-CL ABUT. BRG.	201'-4	213'-10	226'-4	243'-0
WITH STEEL PILES	"A" PILE SPACES		5	6	6	6
	"B" (FT. - IN.)		5'-11	4'-11	4'-11	4'-11
	"C" (FT. - IN.)		2'-5 3/8	2'-6 1/8	2'-6 1/8	2'-6 1/8
	"D" EQUAL SPACES		4	3	3	3
	NO. OF PILES PER ABUT.		8	9	9	9
P _u STRENGTH I DESIGN LOAD (KIPS)			144	127	131	139

NOTE: P_u STRENGTH I DESIGN LOAD (KIPS) IS NOT THE VALUE USED IN THE FIELD FOR DRIVING PILES.



PART SECTION C-C

* NOTE: SEE BARRIER RAIL SHEET FOR DETAILS. REINFORCING BARS 5c3 AND 5c14 ARE INCLUDED IN SUPERSTRUCTURE QUANTITIES.

LATEST REVISION DATE 04-13 APPROVED BY BRIDGE ENGINEER 	 Iowa Department of Transportation Highway Division
	STANDARD DESIGN - 30' ROADWAY, THREE SPAN BRIDGES PRETENSIONED PRESTRESSED CONCRETE BEAM BRIDGES DECEMBER, 2006
	ABUTMENT DETAILS 15° SKEW C BEAMS

H30-12-06