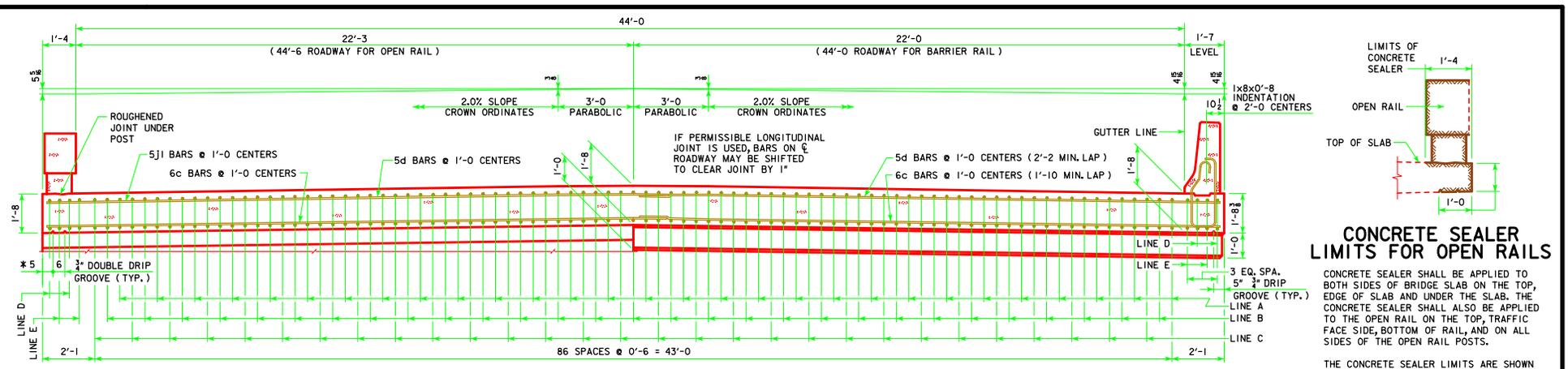


REVISED 04-13 - NOTE FOR BOTTOM OF SLAB BAR CLEARANCE CHANGED TO 1 1/2".



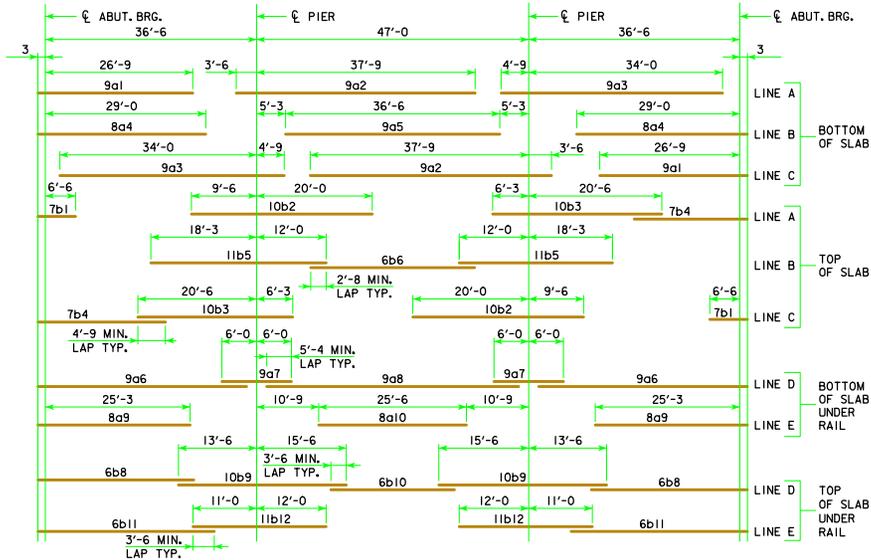
HALF SECTION NEAR ABUTMENT

SLAB CROSS-SECTIONAL AREA FOR OPEN RAIL = 78.61 SQ. FT.

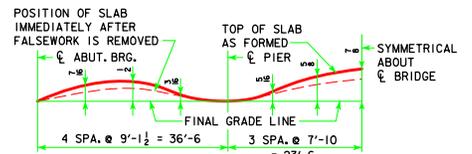
HALF SECTION NEAR PIER

SLAB CROSS-SECTIONAL AREA FOR BARRIER RAIL = 78.66 SQ. FT.

NOTE: TOP LONGITUDINAL REINFORCING STEEL IS TO BE PARALLEL TO AND 2" CLEAR BELOW TOP OF SLAB. BOTTOM LONGITUDINAL REINFORCING STEEL IS TO BE PARALLEL TO AND 1 1/2" CLEAR ABOVE BOTTOM OF SLAB. REINFORCING STEEL IS TO BE SECURELY WIRED IN PLACE AND ADEQUATELY SUPPORTED ON BAR CHAIRS BEFORE CONCRETE IS POURED. I.M. 451.01 REQUIREMENTS SHALL APPLY FOR BAR CHAIRS.



PLACEMENT FOR LONGITUDINAL REINFORCEMENT



FORM CAMBER DIAGRAM

THIS DIAGRAM SHOWS THE FORM CAMBER REQUIRED TO COMPENSATE FOR THE ANTICIPATED ULTIMATE DEAD LOAD DEFLECTION. THE ABOVE DIMENSIONS DO NOT INCLUDE ANY ALLOWANCE FOR FORM DEFLECTION OR FALSEWORK SETTLEMENT.

LATEST REVISION DATE 04-13 APPROVED BY BRIDGE ENGINEER 	 STANDARD DESIGN - 44' ROADWAY, 3 SPAN BRIDGES CONTINUOUS CONCRETE SLAB BRIDGES NOVEMBER, 2006
	SUPERSTRUCTURE DETAILS 120'-0" BRIDGE
	J44-12-06