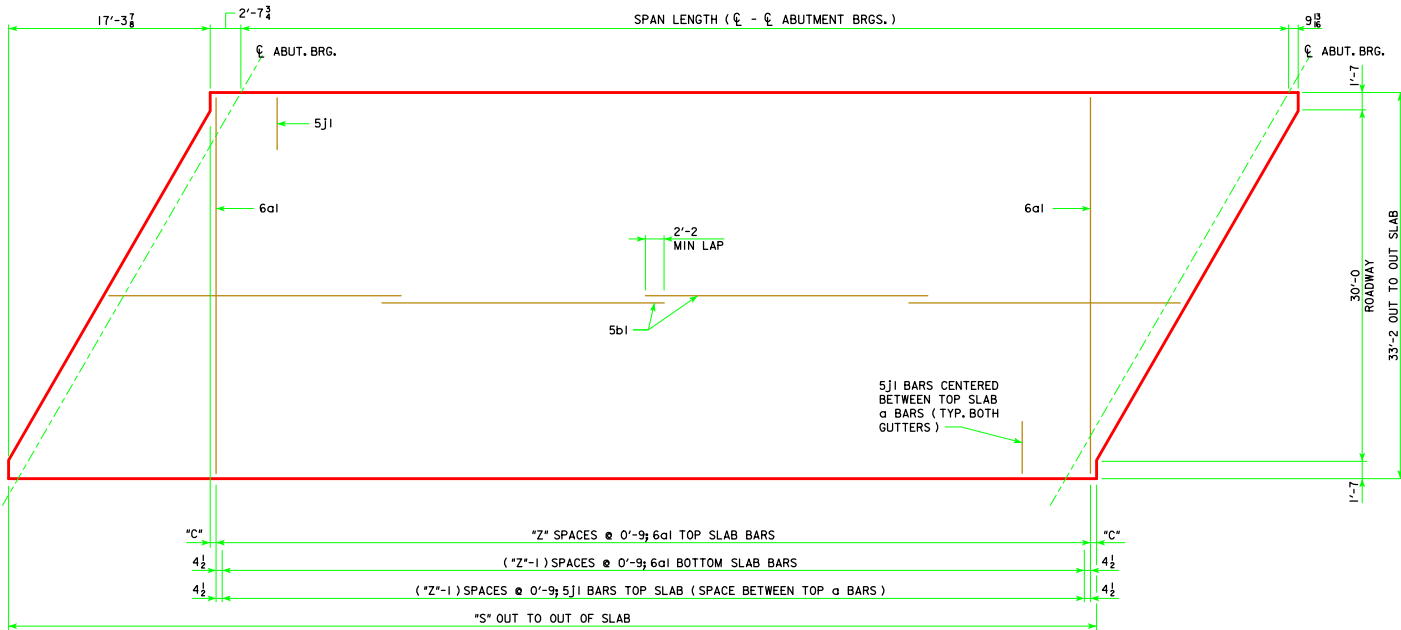
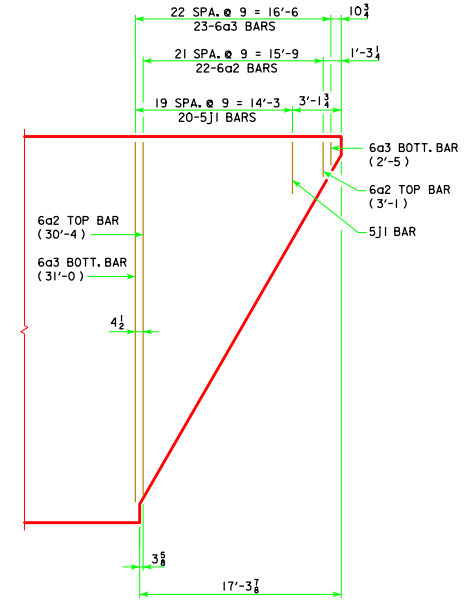


REVISED 05-13 -- REVISION FOR LRFD PILE DESIGN.



**SLAB LAYOUT**  
(LEFT AHEAD SKEW SHOWN, RIGHT AHEAD SKEW SIMILAR)



**END OF SLAB REINFORCING**  
(TYPICAL EACH END OF DECK)

GENERAL DATA		46'-8	55'-0	67'-6	80'-0	90'-0	100'-0	110'-0
SPAN LENGTH (C - C ABUTMENT BRGS.)		46'-8	55'-0	67'-6	80'-0	90'-0	100'-0	110'-0
LOCATION OF EXTREME 6a1 TOP BAR FROM END OF SLAB	"C"	3 3/8	3 3/8	2 3/8	5 3/8	2 3/8	3 3/8	5 3/8
NO. OF SPACES FOR 6a1 TOP BARS	"Z"	43	54	71	87	101	114	127
OUT TO OUT OF SLAB	"S"	50'-1 3/8	58'-5 3/8	70'-11 3/8	83'-5 3/8	93'-5 3/8	103'-5 3/8	113'-5 3/8
VERTICAL CURVE	TOP OF SLAB TO ABUTMENT TOP AT C ABUTMENT BEARING	"U"	3'-8 1/2	3'-8 1/2	4'-3 3/8	4'-9 1/8	5'-6 3/8	5'-7 3/8
STRAIGHT GRADE	TOP OF SLAB TO ABUTMENT TOP AT C ABUTMENT BEARING	"U"	3'-8 3/8	3'-8 3/8	4'-4 3/8	4'-9 3/8	5'-6 3/8	5'-8 1/4
SERVICE D.L. ABUTMENT REACTION (D.L. + F.W.S.) SERVICE LOADS	KIPS	314.7	341.8	406.9	521.6	595.9	637.0	678.1
SERVICE L.L. ABUTMENT REACTION (HL-93) NO IMPACT SERVICE LOADS	KIPS	152.8	162.4	175.0	186.0	194.4	202.2	209.8

LATEST REVISION DATE 05-13	APPROVED BY BRIDGE ENGINEER <i>Thomas E. M. Donnell</i>		
		STANDARD DESIGN - 30' ROADWAY, SINGLE SPAN BRIDGE <b>PRETENSIONED PRESTRESSED CONCRETE BEAM BRIDGES</b> APRIL, 2012	
		<b>SUPERSTRUCTURE</b> 30° SKEW	<b>H30SI-19-12</b>