

CONCRETE SEALER SHALL BE APPLIED TO BOTH SIDES OF BRIDGE SLAB ON THE TOP, EDGE OF SLAB AND UNDER THE SLAB. THE CONCRETE SEALER SHALL ALSO BE APPLIED TO THE OPEN RAIL ON THE TOP, TRAFFIC FACE SIDE, BOTTOM OF RAIL, AND ON ALL SIDES OF THE OPEN RAIL POSTS.

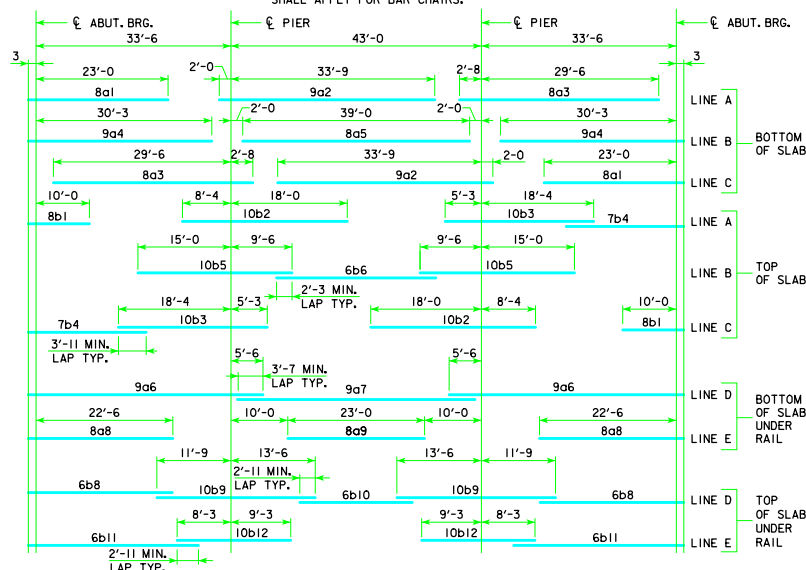
THE CONCRETE SEALER LIMITS ARE SHOWN IN THE DETAIL AND SHALL APPLY TO THE FULL LENGTH OF BRIDGE. CONCRETE SEALER SHALL BE APPLIED IN ACCORDANCE WITH ARTICLE 2403.03, P. 3, OF THE STANDARD SPECIFICATIONS.

* NOTE: DOUBLE DRIP GROOVES FOR OPEN RAIL OPTION ONLY.

SLAB CROSS-SECTIONAL AREA
FOR OPEN RAIL = 51.14 SQ. FT.

NOTE: TOP LONGITUDINAL REINFORCING STEEL IS TO BE PARALLEL TO AND 2" CLEAR BELOW TOP OF SLAB. BOTTOM LONGITUDINAL REINFORCING STEEL IS TO BE PARALLEL TO AND 1" CLEAR ABOVE BOTTOM OF SLAB. REINFORCING STEEL IS TO BE SECURELY WIRED IN PLACE AND ADEQUATELY SUPPORTED ON BAR CHAIRS BEFORE CONCRETE IS POURED. I.M. 451.01 REQUIREMENTS SHALL APPLY FOR BAR CHAIRS.

SLAB CROSS-SECTIONAL AREA
FOR BARRIER RAIL = 51.19 SQ. FT.



POSITION OF SLAB IMMEDIATELY AFTER FALSEWORK IS REMOVED

ABUT. BRG.

TOP OF SLAB AS FORMED

PIER

BRIDGE



FINAL GRADE LINE

4 SPA. @ $8' - 4\frac{1}{2}" = 33' - 6"$

3 SPA. @ $7' - 2"$

SYMMETRICAL ABOUT ϵ BRIDGE

THIS DIAGRAM SHOWS THE FORM CAMBER REQUIRED TO COMPENSATE FOR THE ANTICIPATED ULTIMATE DEAD LOAD DEFLECTION. THE ABOVE DIMENSIONS DO NOT INCLUDE ANY ALLOWANCE FOR FORM DEFLECTION OR FALSEWORK SETTLEMENT.

06-12 LATEST REVISION DATE	 Iowa Department of Transportation <i>Highway Division</i>	STANDARD DESIGN - 30' ROADWAY, 3 SPAN BRIDGES CONTINUOUS CONCRETE SLAB BRIDGES NOVEMBER, 2006	
		APPROVED BY BRIDGE ENGINEER 	J30-10B-06