

9-Apr-20 Corrections:

- **Slide 40: Bridge Deck SF estimate correction from 405,675 to 367,633**
- **Slide 76: Proposed Scheduling SP to include cash flow, but not full resource loading**



AGC MEETING – MARCH 23, 2020

Base PROJECT NUMBER IMN-080-6(235)239--0E-52

All information herein is Preliminary and Subject to Change

GREETING AND INTRO



Linda Narigon

GREETING AND INTRO



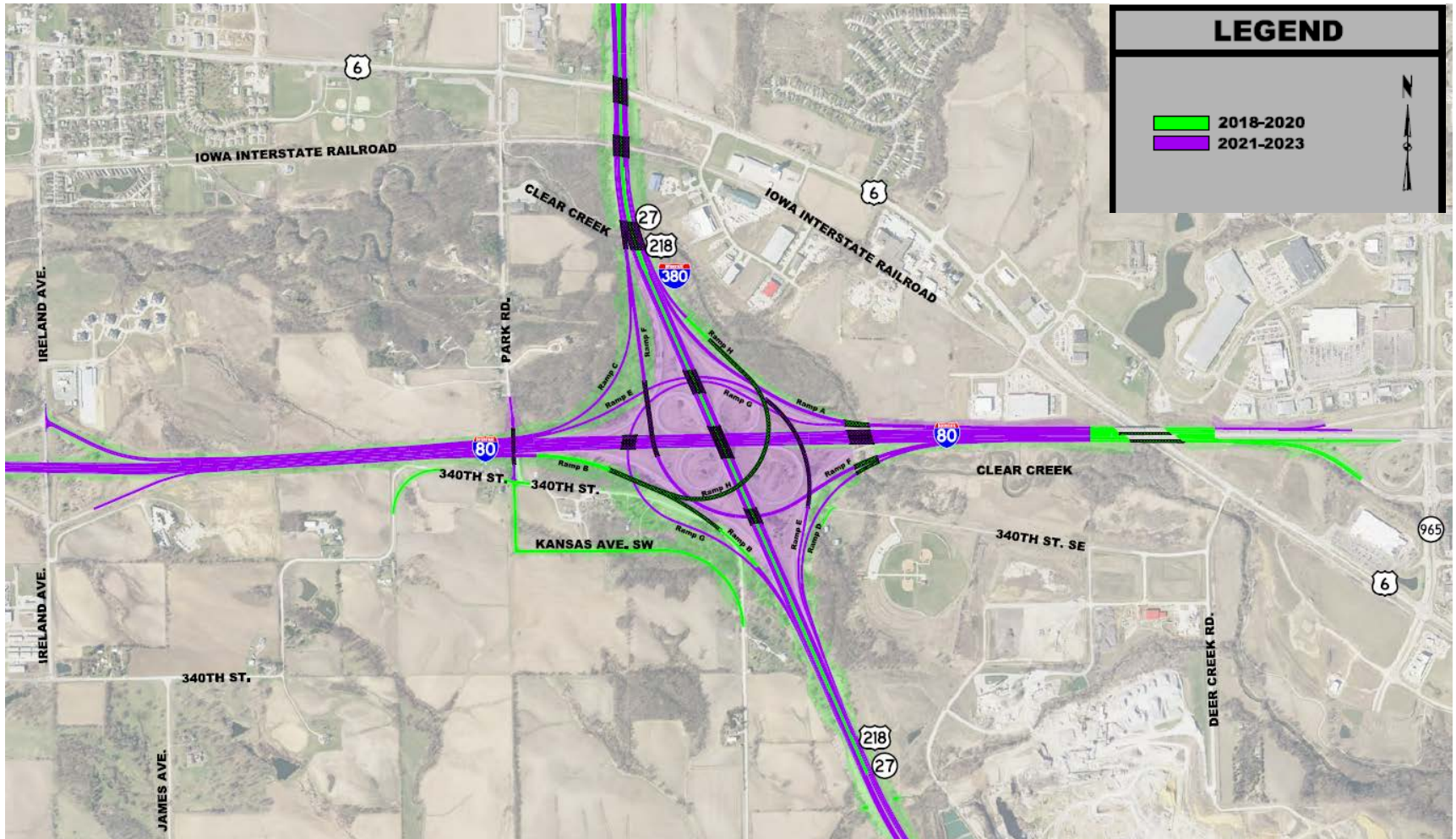
- » Greeting
- » Purpose
- » Introductions

MEETING GOALS



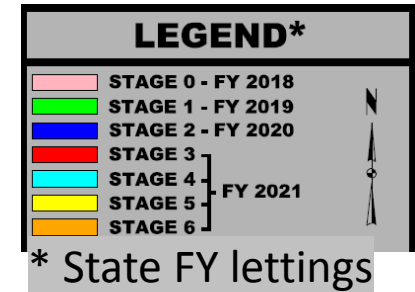
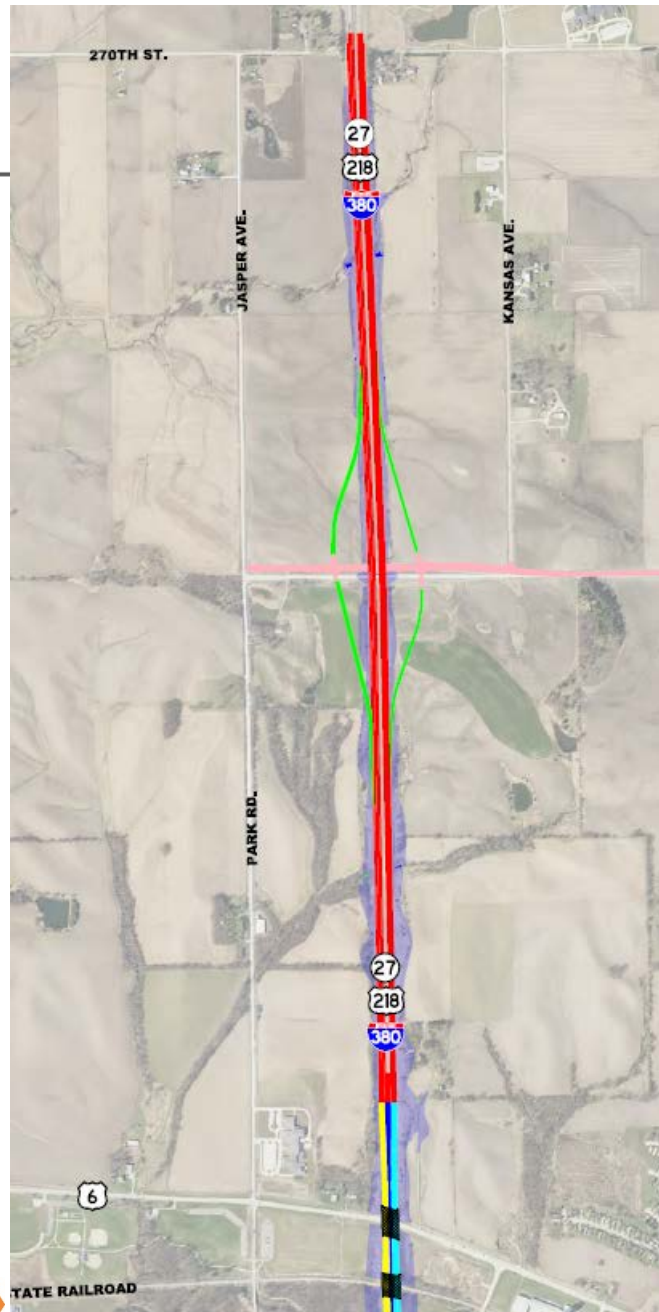
- » Overview of Remaining Program
 - » SFY21 (July 2020) Letting
 - » INFRA Grant
 - » Overview
 - » FY19 and FY20 Current Construction Projects
- » Road, Bridge and Contract's Presentations
- » HDR as GEC in support of DOT
- » Question and Answer session
- » Closing information including submittal of additional questions and comments

WORK OVERVIEW

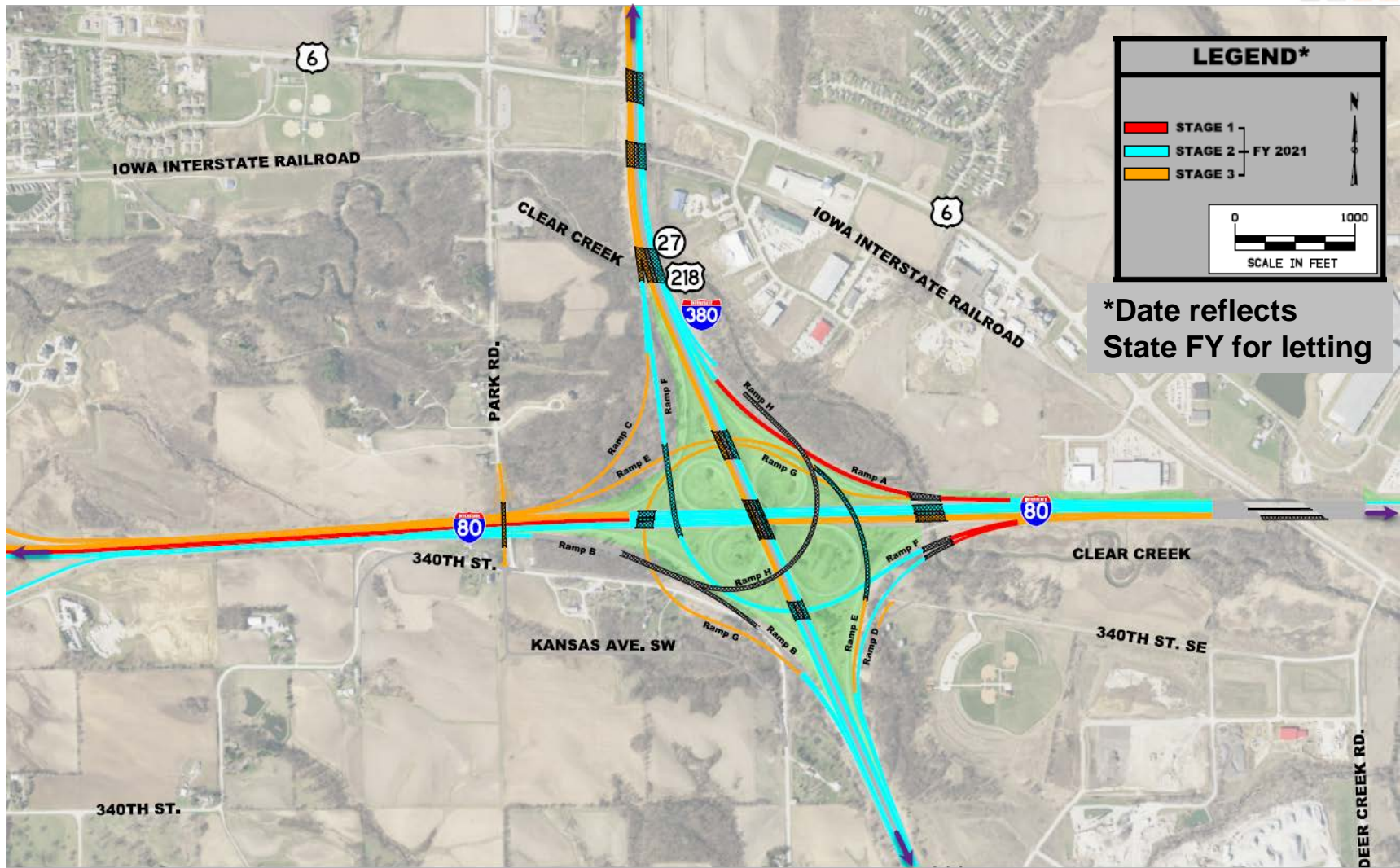


WORK OVERVIEW

NB and SB I-380 staged to be completed in the same time period and completed before fall 2022. Coordinated with work to the south.



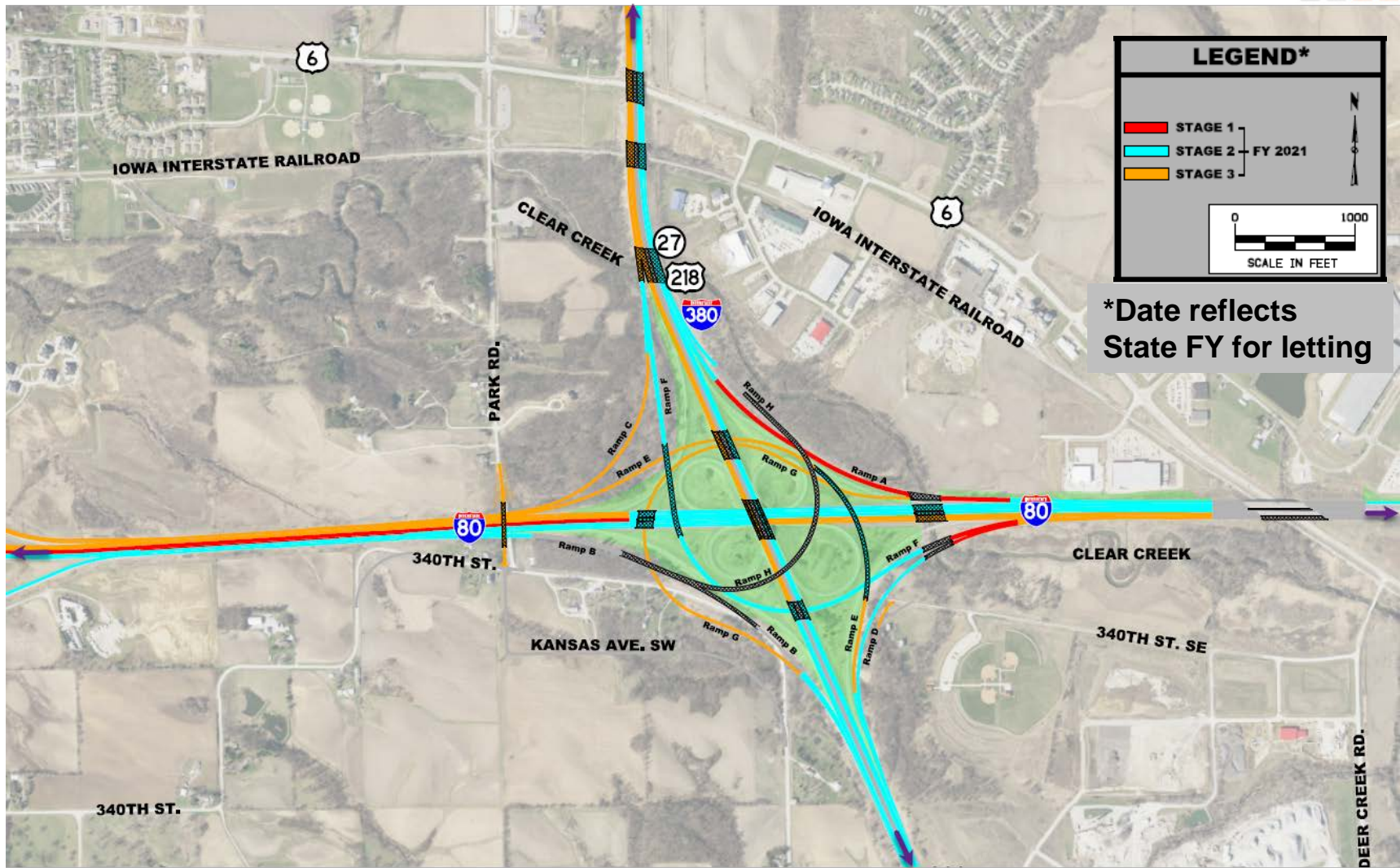
STAGING OVERVIEW – ZOOMED INTO INTERCHANGE AREA



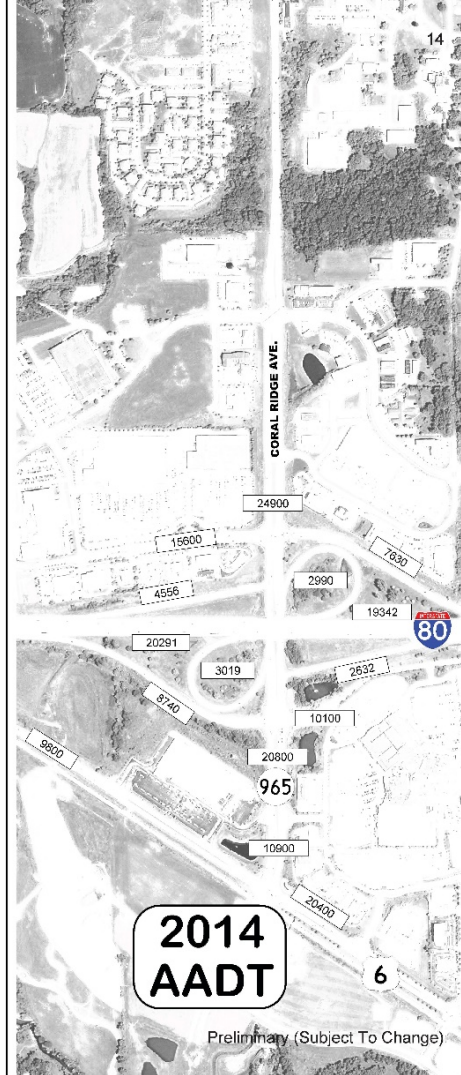
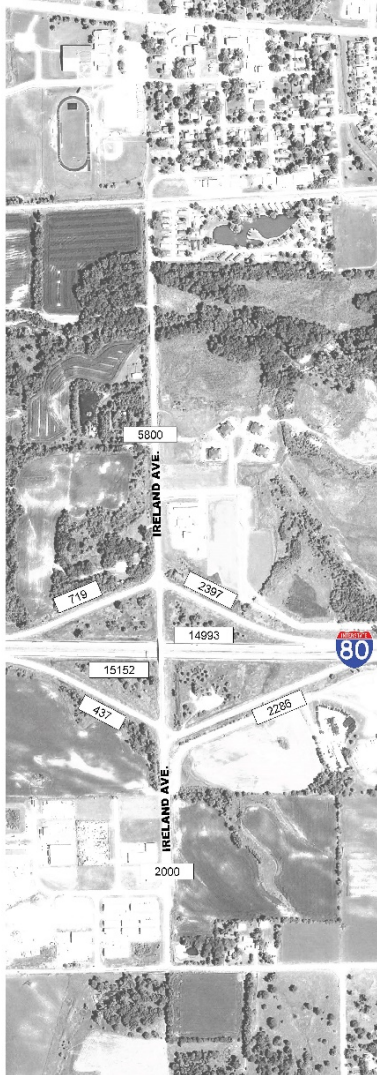


Jason Holst

STAGING OVERVIEW – ZOOMED INTO INTERCHANGE AREA



TRAFFIC



**2014
AADT**

Preliminary (Subject To Change)

HIGH LEVEL QUANTITIES



» Roadway High Level Quantities

FY21 Estimate Quantities by Year							
Year	Earthwork		Pavement		Subbase Material		
	Excavation (CY)	Embankment (CY)	Permanent (SY)	Temporary (SY)	Granular Subbase (CY)	Special Backfill (CY)	Modified Subbase (CY)
Stage 1 Fall 2020 to Spring 2021	65,000	150,000	90,000	12,000	6,000	15,000	23,000
Stage 2 - 2021	400,000	175,000	350,000	30,000	25,000	50,000	69,000
Stage 3 - 2022	200,000	60,000	185,000	2,000	-	-	60,000

- » 80,000 CY Embankment, Contractor Furnish
- » 19,000 CY Topsoil, Contractor Furnish
- » Class 10 Waste is being finalized to determine how much and where it can remain in the infields.

STAGING - GENERAL



- » Staging scrolls will be provided as part of the contract documents
- » Some changes made since the October AGC update meeting
 - » Showing opportunities for earlier work
 - » Piers that can be constructed earlier
 - » Notes when early work isn't possible
 - » TBR bid items
 - » Extra bid item for the small shifts
 - » Tabulated in plans
 - » Additional field adjustments may be necessary

STAGING - WINTER

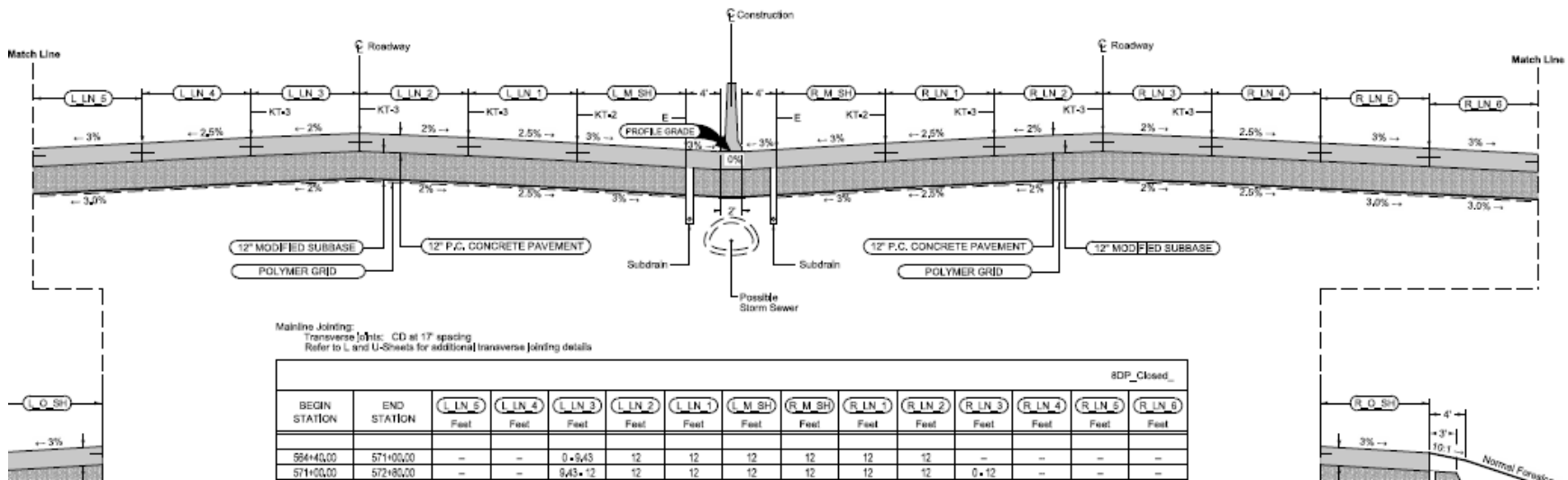


- » Winter configurations
 - » Winter chutes allowed.
 - » Extra TBR shifts
 - » To allow for extra shoulder width when possible
 - » Winter traffic configuration will need to be coordinated between Contractor and the Engineer

STAGING - PAVING



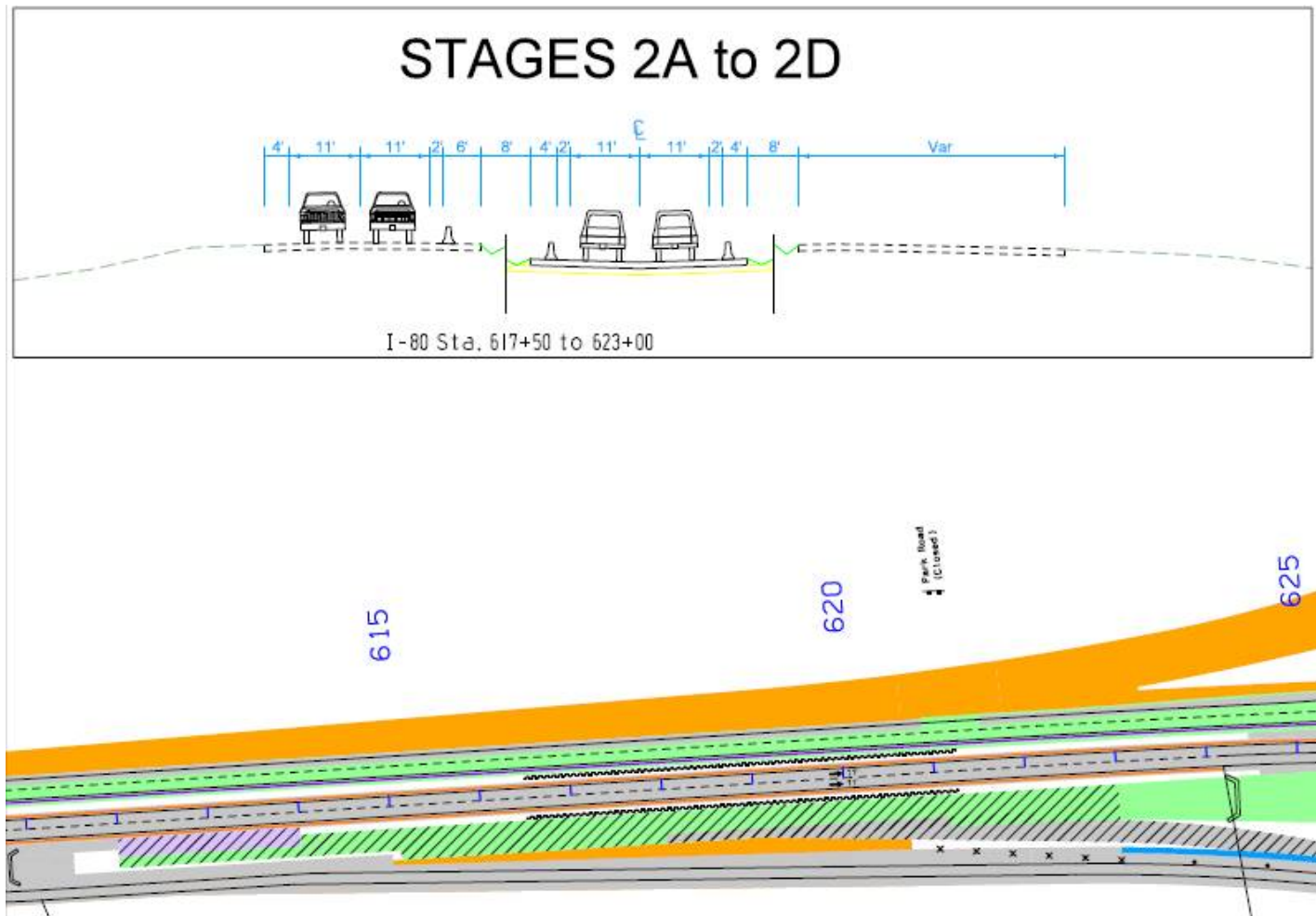
- » Width behind TBR for paving. Adjacent to median.



STAGING - PAVING



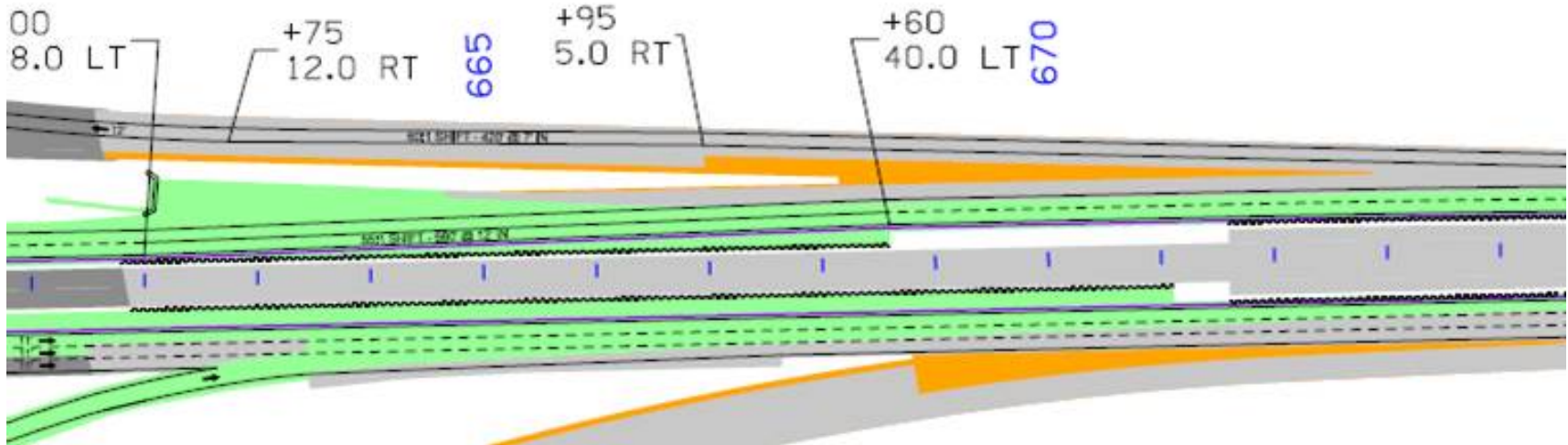
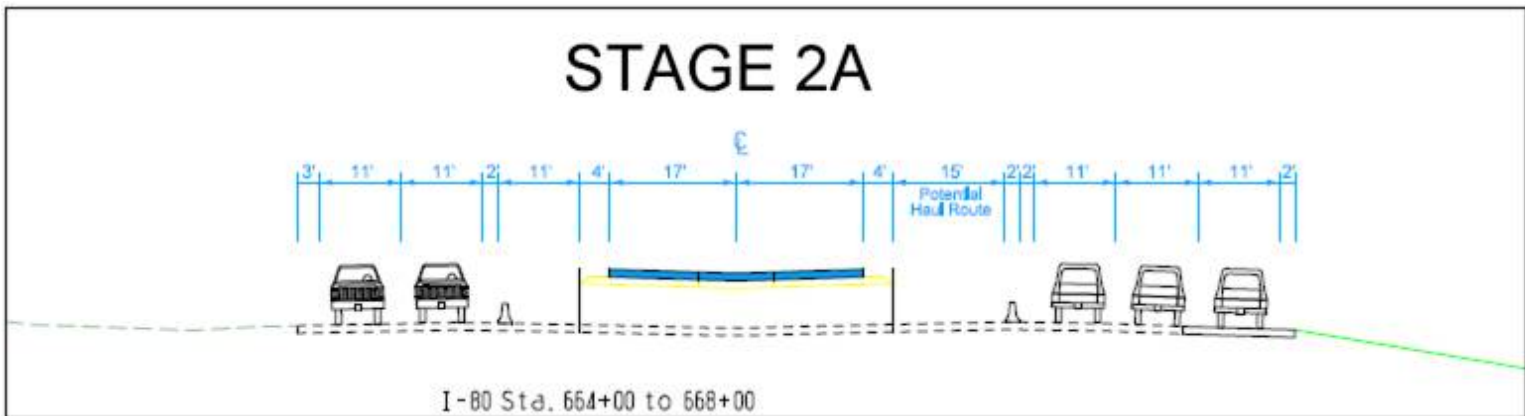
- » Width behind TBR for paving



STAGING - PAVING



» Width behind TBR for paving

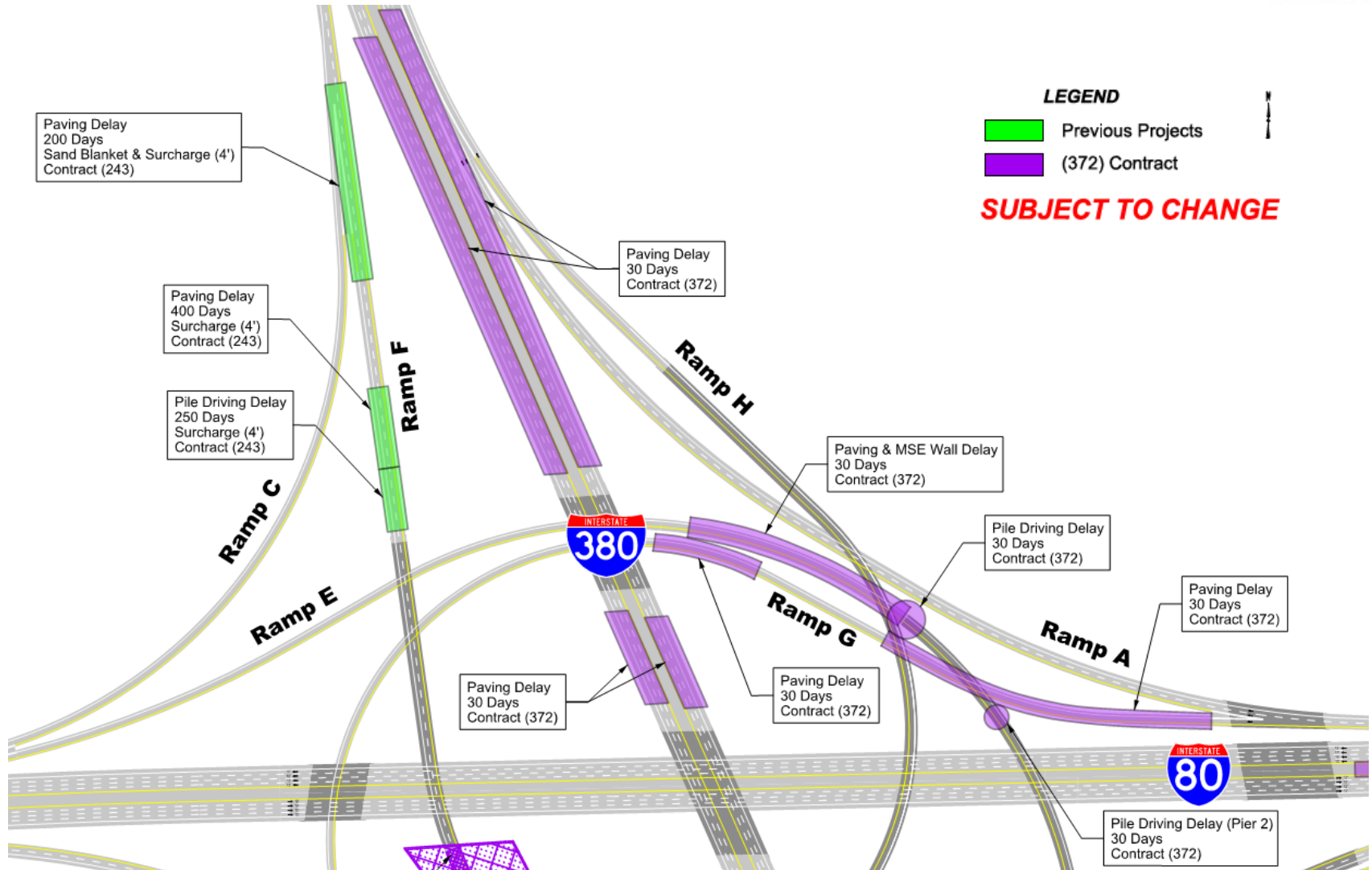


EARTHWORK

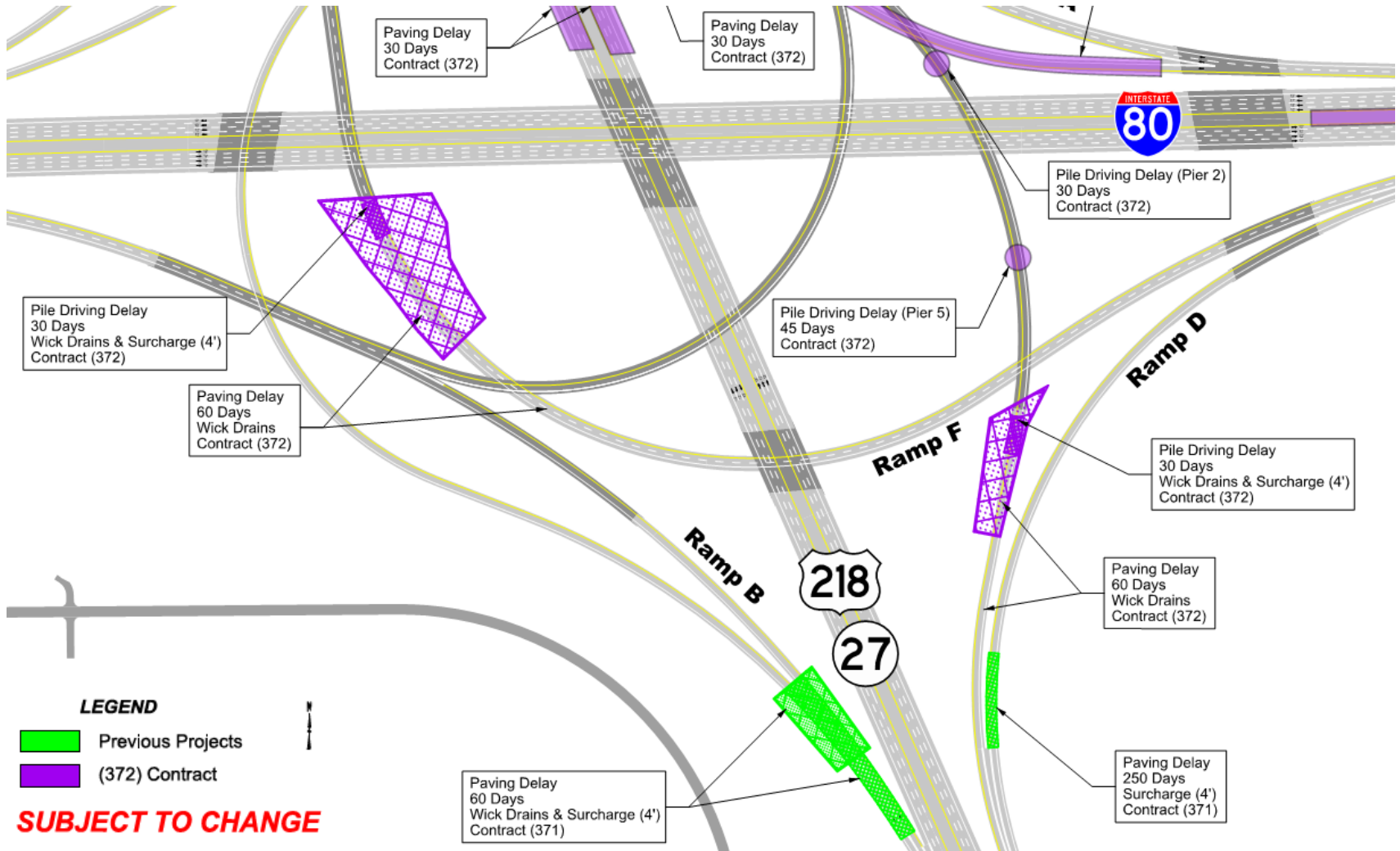


- » Surface models – approach
- » Holding time and geotech info on the next slides
 - » Automated settlement readings for some sites

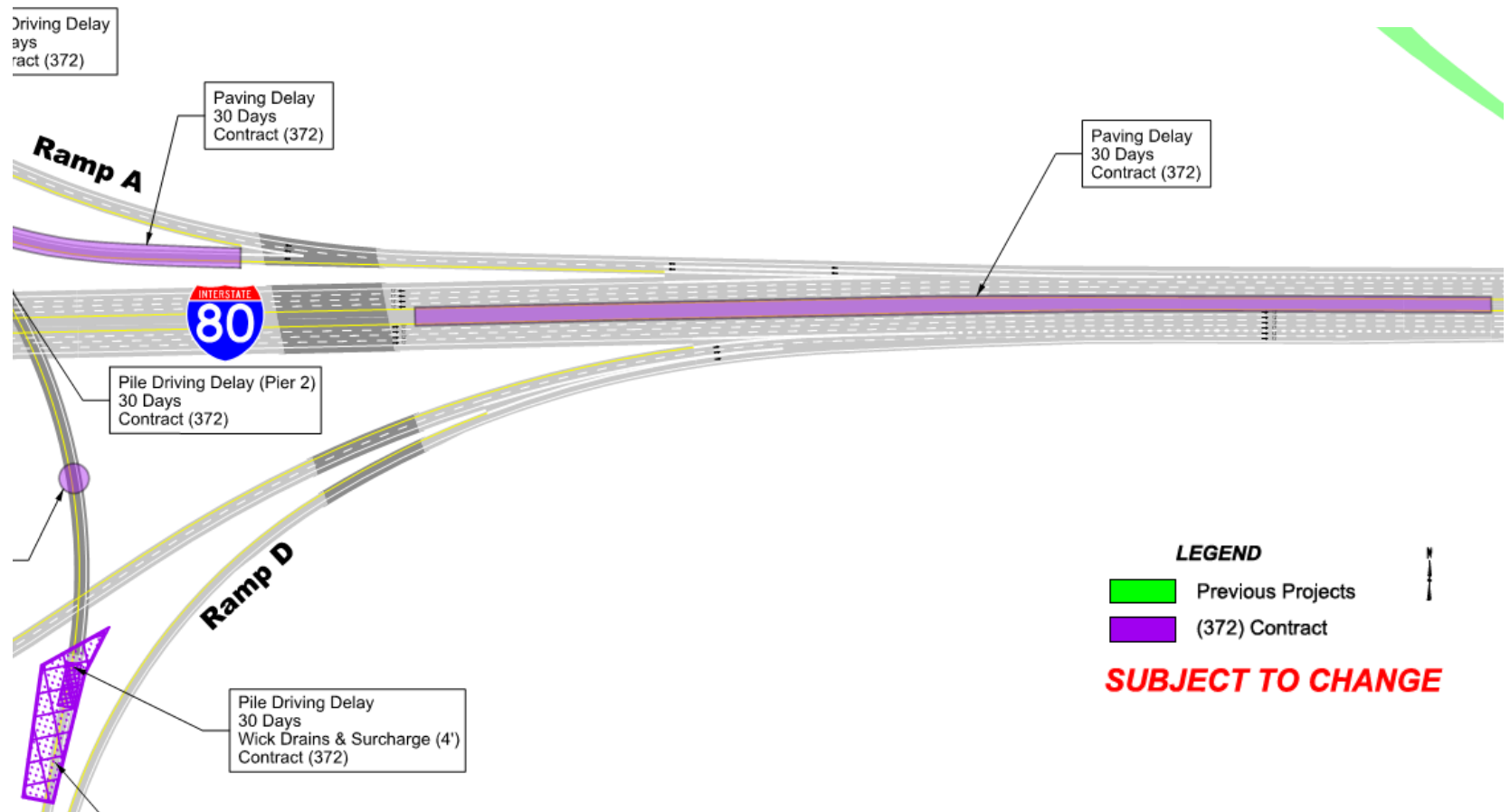
GEOTECH – HOLDING TIMES



GEOTECH – HOLDING TIMES



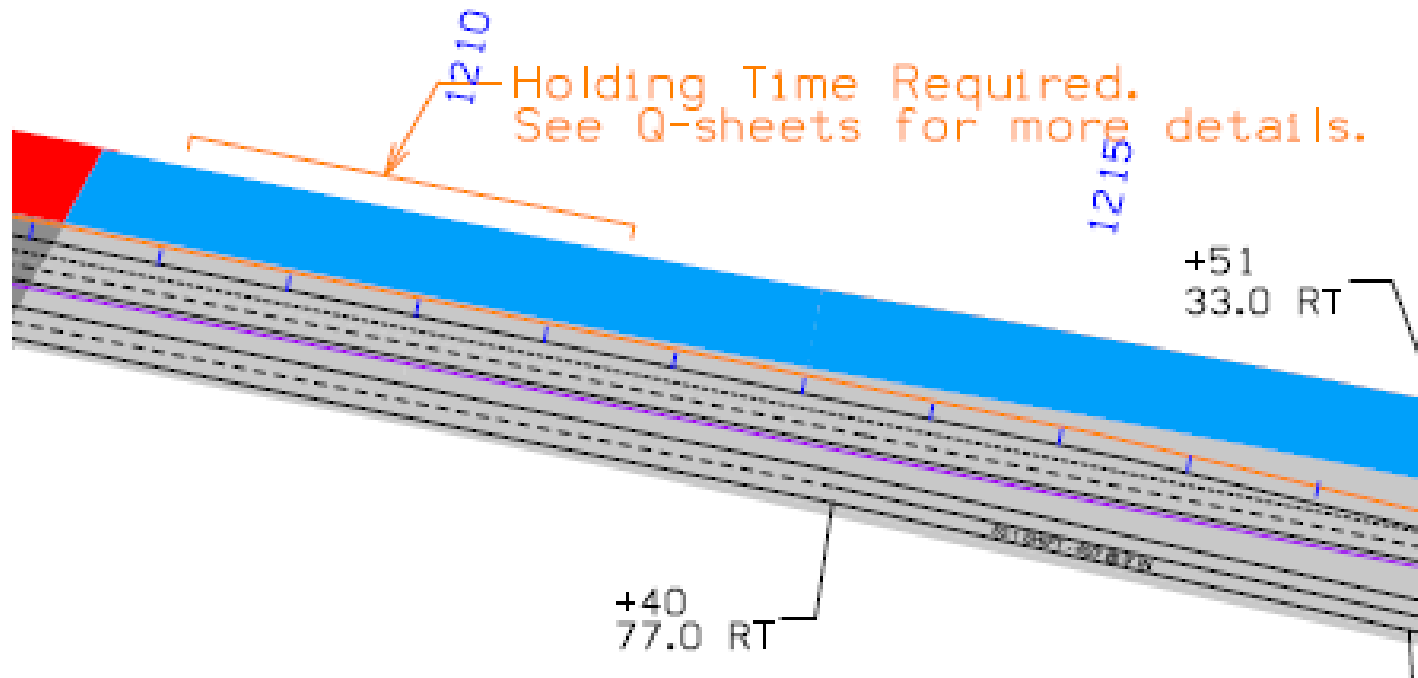
GEOTECH – HOLDING TIMES



GEOTECH – HOLDING TIMES



- » Holding times shown on Staging Scrolls



GEOTECH – HOLDING TIMES

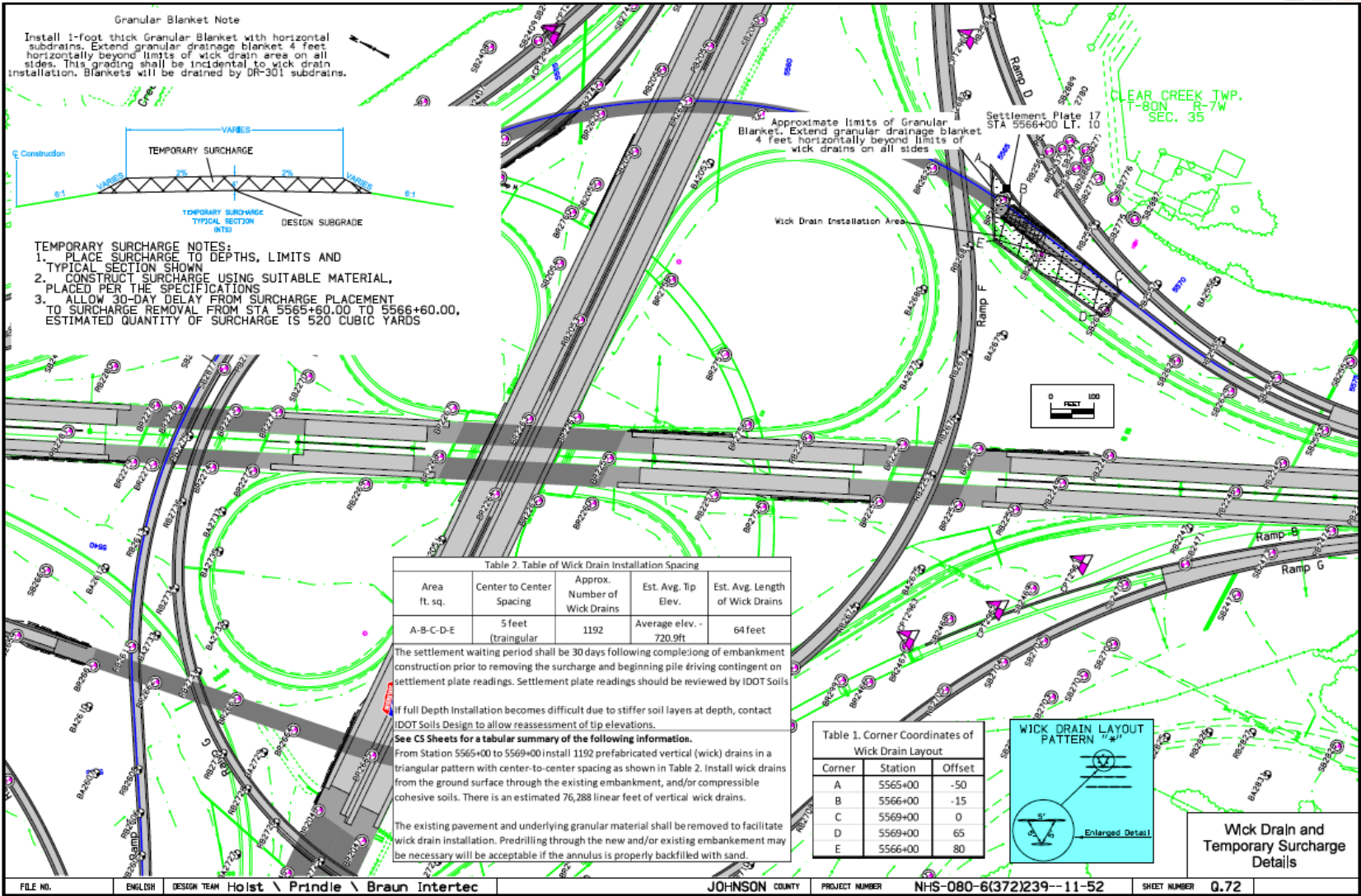


Paving Delay Holding Times				
Alignment	Direction	Stations Begin	Station End	Delay
I-80	Median	662+50	687+00	30 days
I-380	NB	1165+75	1168+50	30 days
I-380	NB	1172+95	1186+15	30 days
I-380	NB	1191+25	1197+35	30 days
I-380	NB	1201+05	1203+40	60 days
I-380	NB	1207+85	1211+50	60 days
I-380	SB	1166+00	1168+80	30 days
I-380	SB	1173+10	1186+75	30 days
I-380	SB	1191+60	1197+55	30 days
I-380	SB	1201+20	1203+70	30 days
I-380	SB	1203+05	1211+50	60 days
Ramp E		5565+60	5571+00	60 days
Ramp E		5545+50	5552+00	30 days
Ramp F		6532+50	6540+00	60 days
Ramp G		7542+00	7545+00	30 days
Ramp G		7549+00	7558+45	30 days

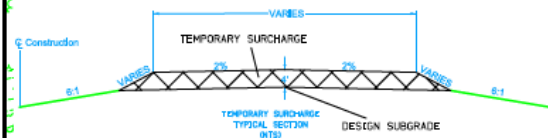
Pile Driving Delay Holding Times			
Alignment	Direction	Location	Delay
Ramp E		S. Abutment	30 days
Ramp F		S. Abutment	30 days

Paving and Pile
Driving Delays

GEOTECH – WICK DRAINS



Granular Blanket Note
 Install 1-foot thick Granular Blanket with horizontal subdrains. Extend granular drainage blanket 4 feet horizontally beyond limits of wick drain area on all sides. This grading shall be incidental to wick drain installation. Blankets will be drained by DR-301 subdrains.



- TEMPORARY SURCHARGE NOTES:**
1. PLACE SURCHARGE TO DEPTHS, LIMITS AND TYPICAL SECTION SHOWN
 2. CONSTRUCT SURCHARGE USING SUITABLE MATERIAL, PLACED PER THE SPECIFICATIONS
 3. ALLOW 30-DAY DELAY FROM SURCHARGE PLACEMENT TO SURCHARGE REMOVAL FROM STA 5565+60.00 TO 5566+60.00. ESTIMATED QUANTITY OF SURCHARGE IS 520 CUBIC YARDS

Approximate limits of Granular Blanket. Extend granular drainage blanket 4 feet horizontally beyond limits of wick drains on all sides



Table 2. Table of Wick Drain Installation Spacing

Area ft. sq.	Center to Center Spacing	Approx. Number of Wick Drains	Est. Avg. Tip Elev.	Est. Avg. Length of Wick Drains
A-B-C-D-E	5 feet (triangular)	1192	Average elev. - 720.9ft	64 feet

The settlement waiting period shall be 30 days following completion of embankment construction prior to removing the surcharge and beginning pile driving contingent on settlement plate readings. Settlement plate readings should be reviewed by IDOT Soils

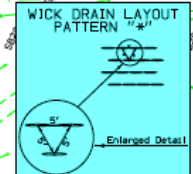
If full Depth Installation becomes difficult due to stiffer soil layers at depth, contact IDOT Soils Design to allow reassessment of tip elevations.

See CS Sheets for a tabular summary of the following information.
 From Station 5565+00 to 5569+00 install 1192 prefabricated vertical (wick) drains in a triangular pattern with center-to-center spacing as shown in Table 2. Install wick drains from the ground surface through the existing embankment, and/or compressible cohesive soils. There is an estimated 76,288 linear feet of vertical wick drains.

The existing pavement and underlying granular material shall be removed to facilitate wick drain installation. Predrilling through the new and/or existing embankment may be necessary will be acceptable if the annulus is properly backfilled with sand.

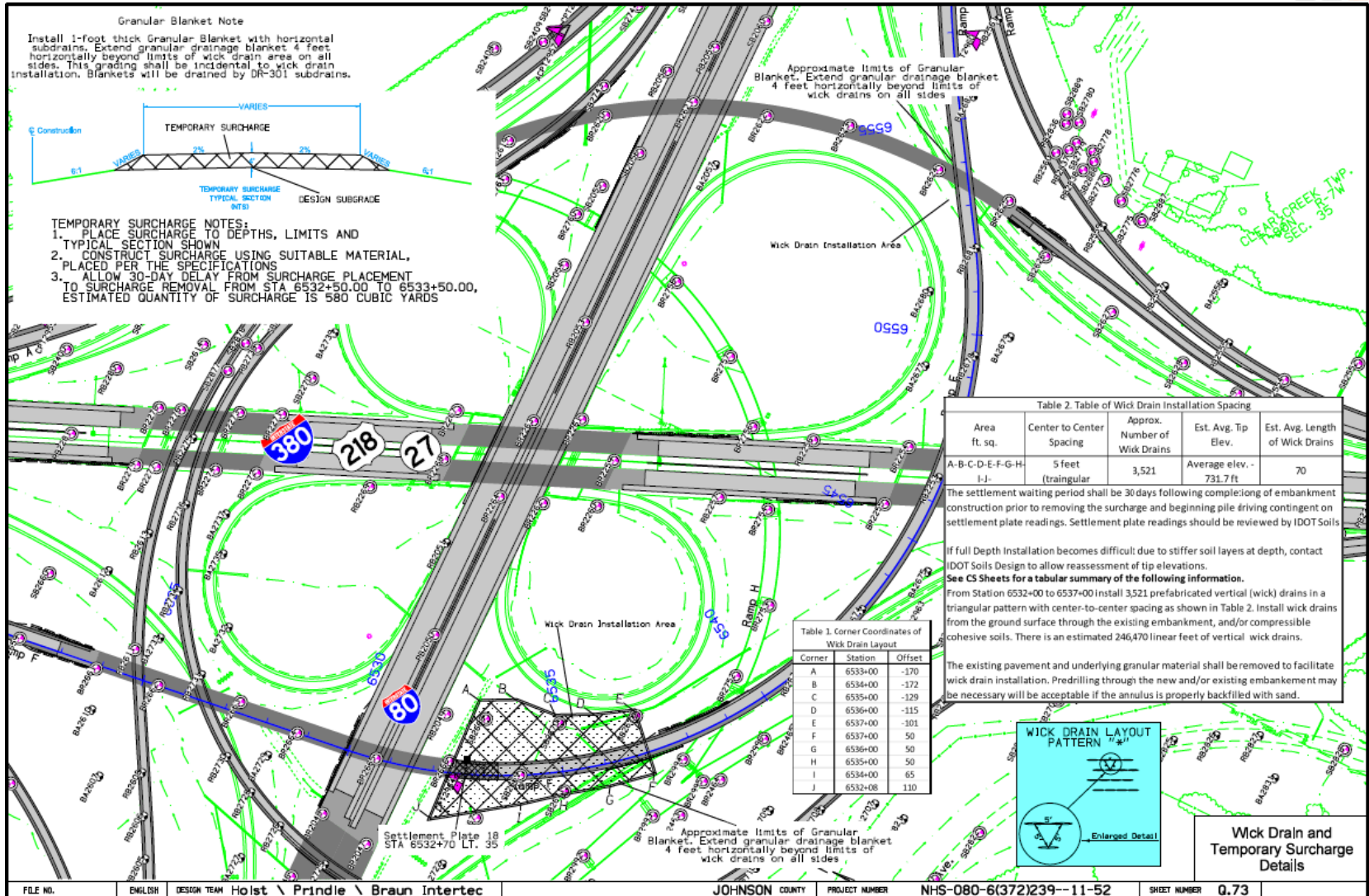
Table 1. Corner Coordinates of Wick Drain Layout

Corner	Station	Offset
A	5565+00	-50
B	5566+00	-15
C	5569+00	0
D	5569+00	65
E	5566+00	80



Wick Drain and Temporary Surcharge Details

GEOTECH – WICK DRAINS

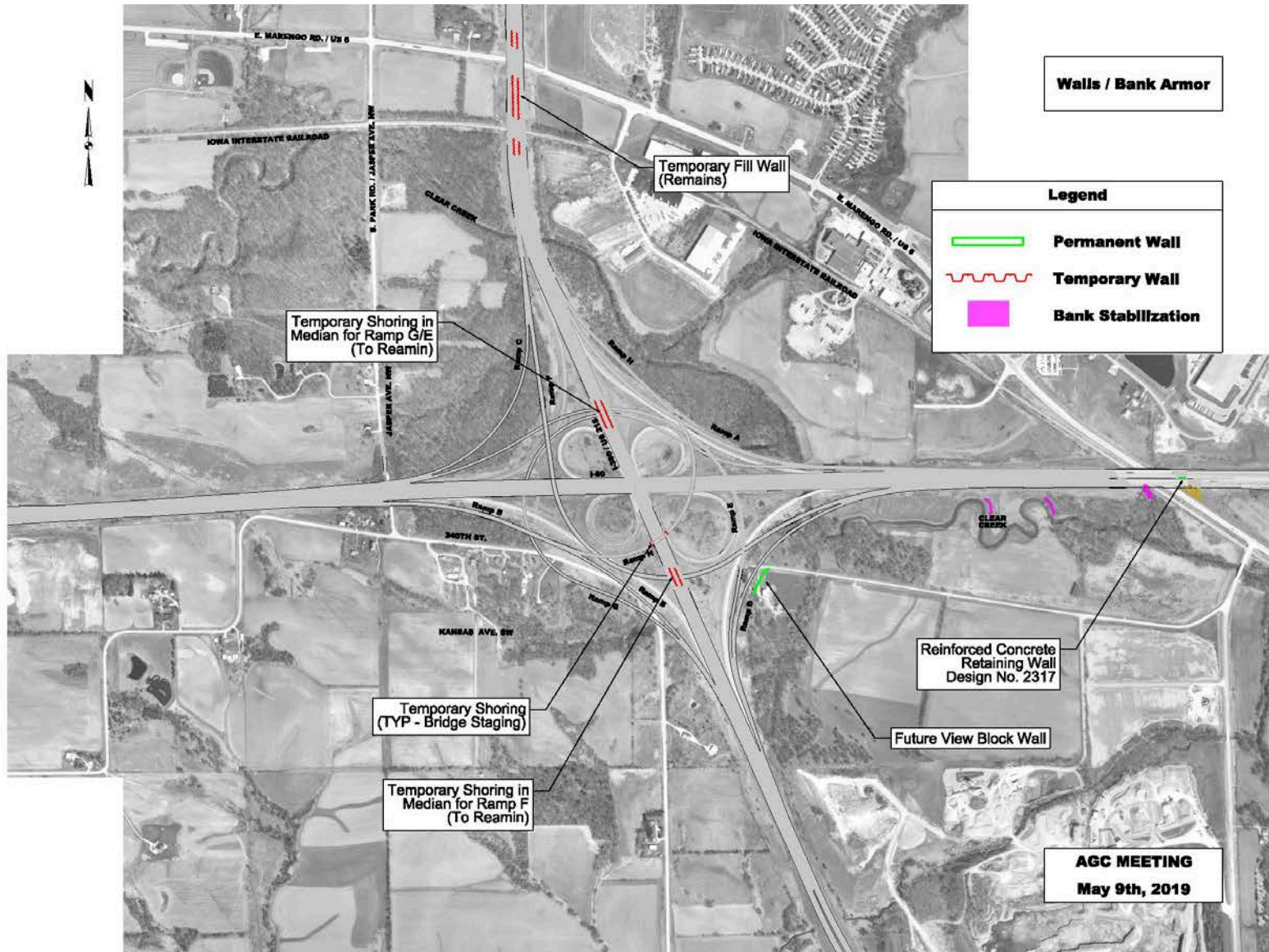


RETAINING WALLS

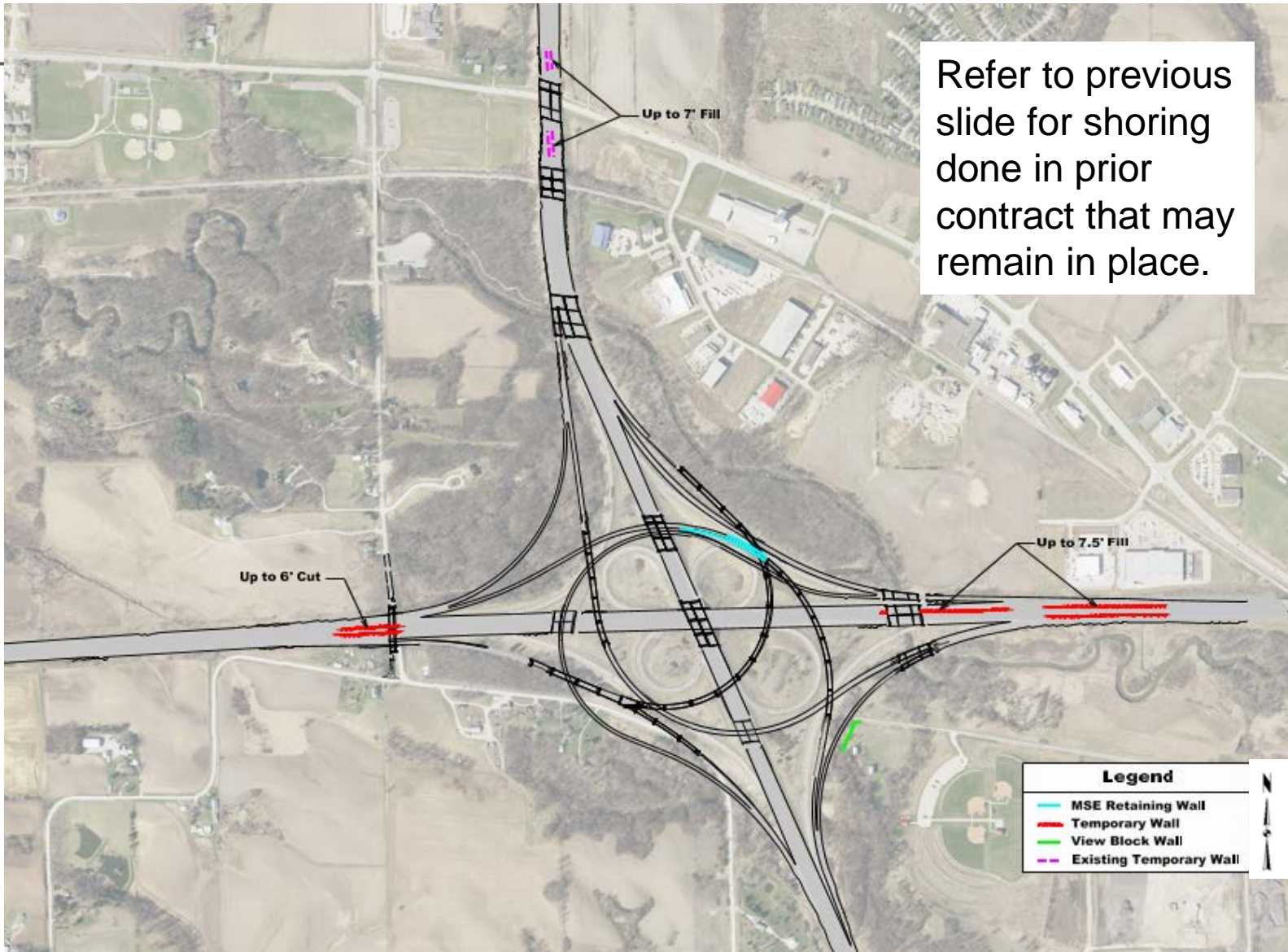


- » Permanent Retaining Walls
 - » MSE Wall
 - » Noise Wall / View Block Wall
- » “Temporary” Walls
 - » Fill Walls
 - » Temporary MSE Wall
 - » Cut Walls
 - » Buy America for piling left in place

RETAINING WALLS – EXISTING

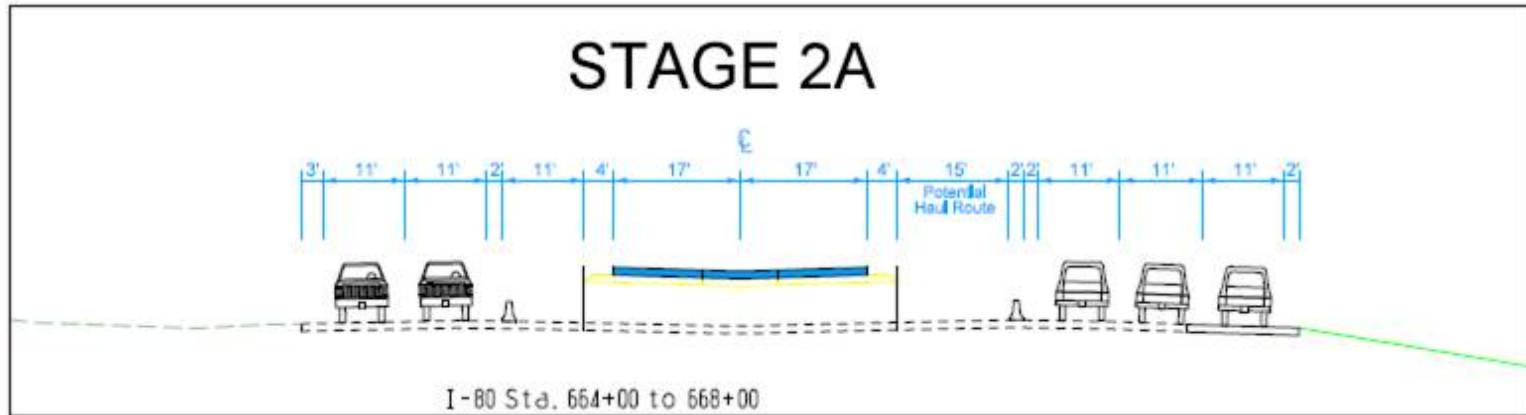


RETAINING WALLS – PROPOSED

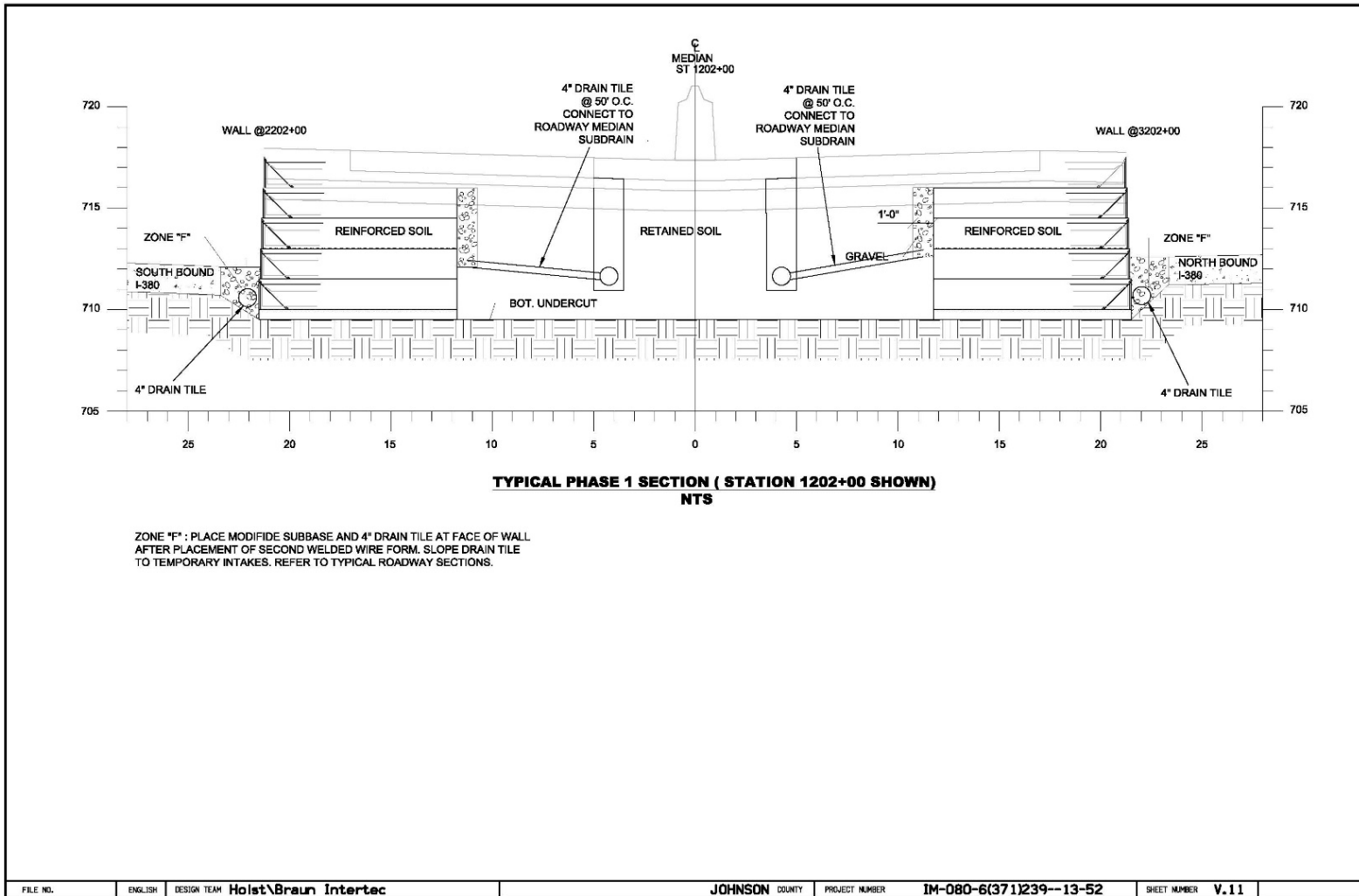


Refer to previous slide for shoring done in prior contract that may remain in place.

RETAINING WALLS – I-80 TYPICAL



RETAINING WALLS



STAGING – INGRESS / EGRESS



Access Route Legend:

Possible Haul Route 

Work Zone Ingress 

Work Zone Egress 

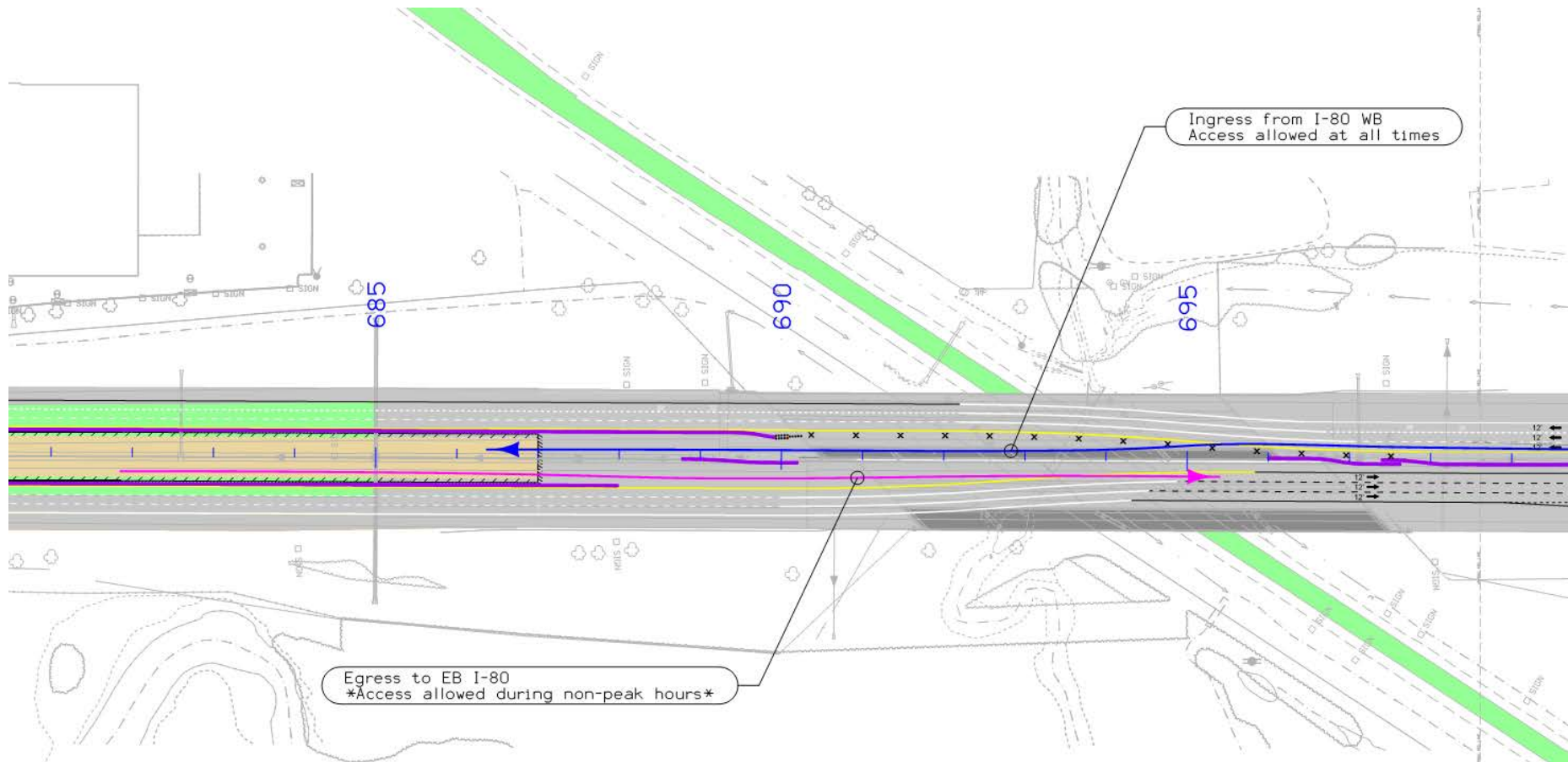
Peak hours are defined as:

- o Monday 6:00 a.m. to 9:00 a.m. and 3:00 p.m. to 6:00 p.m.
- o Tuesday 6:00 a.m. to 9:00 a.m. and 3:00 p.m. to 6:00 p.m.
- o Wednesday 6:00 a.m. to 9:00 a.m. and 3:00 p.m. to 6:00 p.m.
- o Thursday 6:00 a.m. to 9:00 a.m. and 3:00 p.m. to 6:00 p.m.
- o Friday 6:00 a.m. to 9:00 a.m. and 3:00 p.m. to 6:00 p.m.

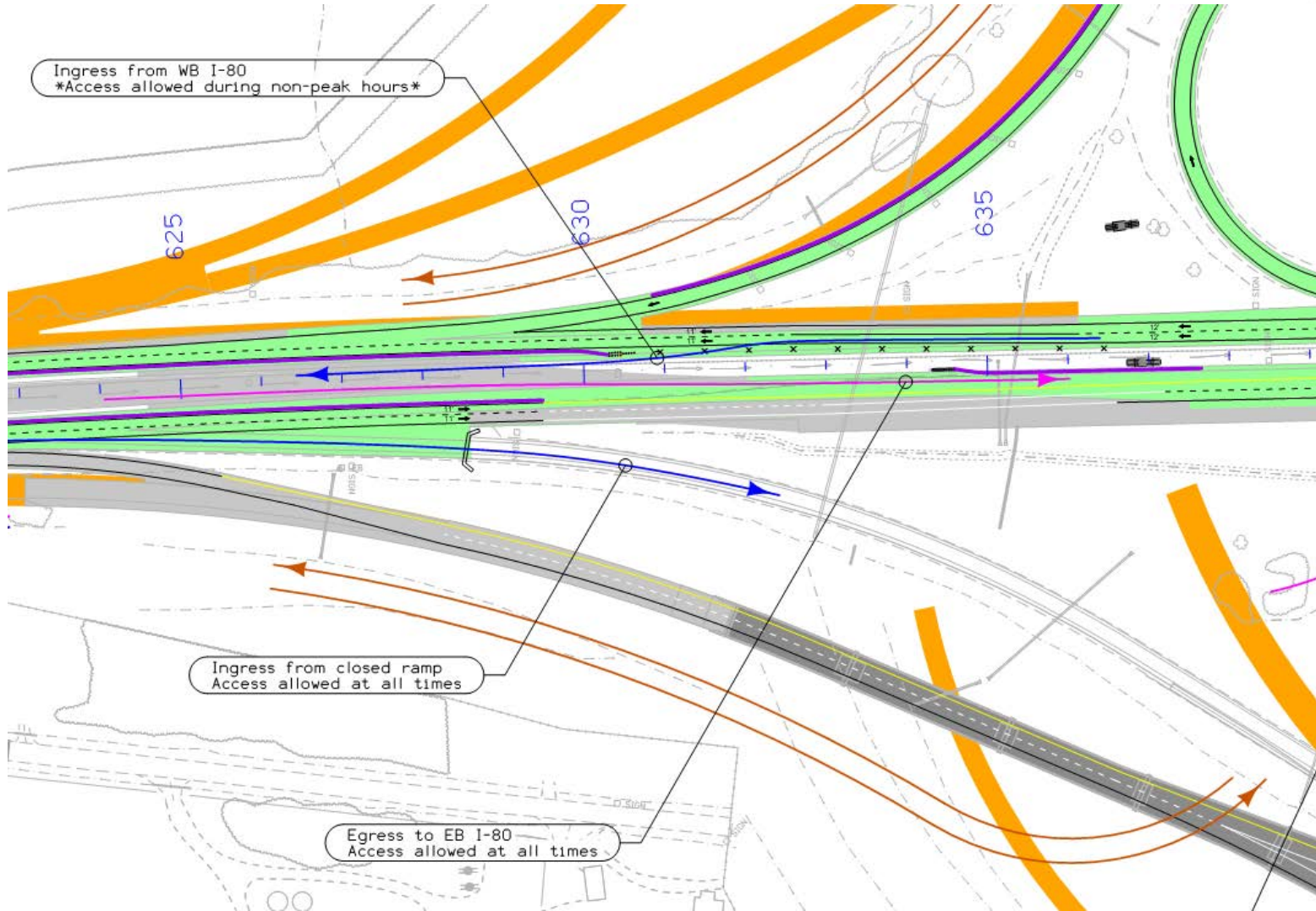
» Notes to provide clarity and intent:

- » Access allowed at all times.
- » Access allowed during non-peak hour traffic, requires shoulder closure per TC-402.
- » Access allowed during non-peak hour traffic, requires partial ramp narrowing per TC-416.
- » Access only allowed at night, per the J.1 lane closures times, requires lane closure per TC-418.

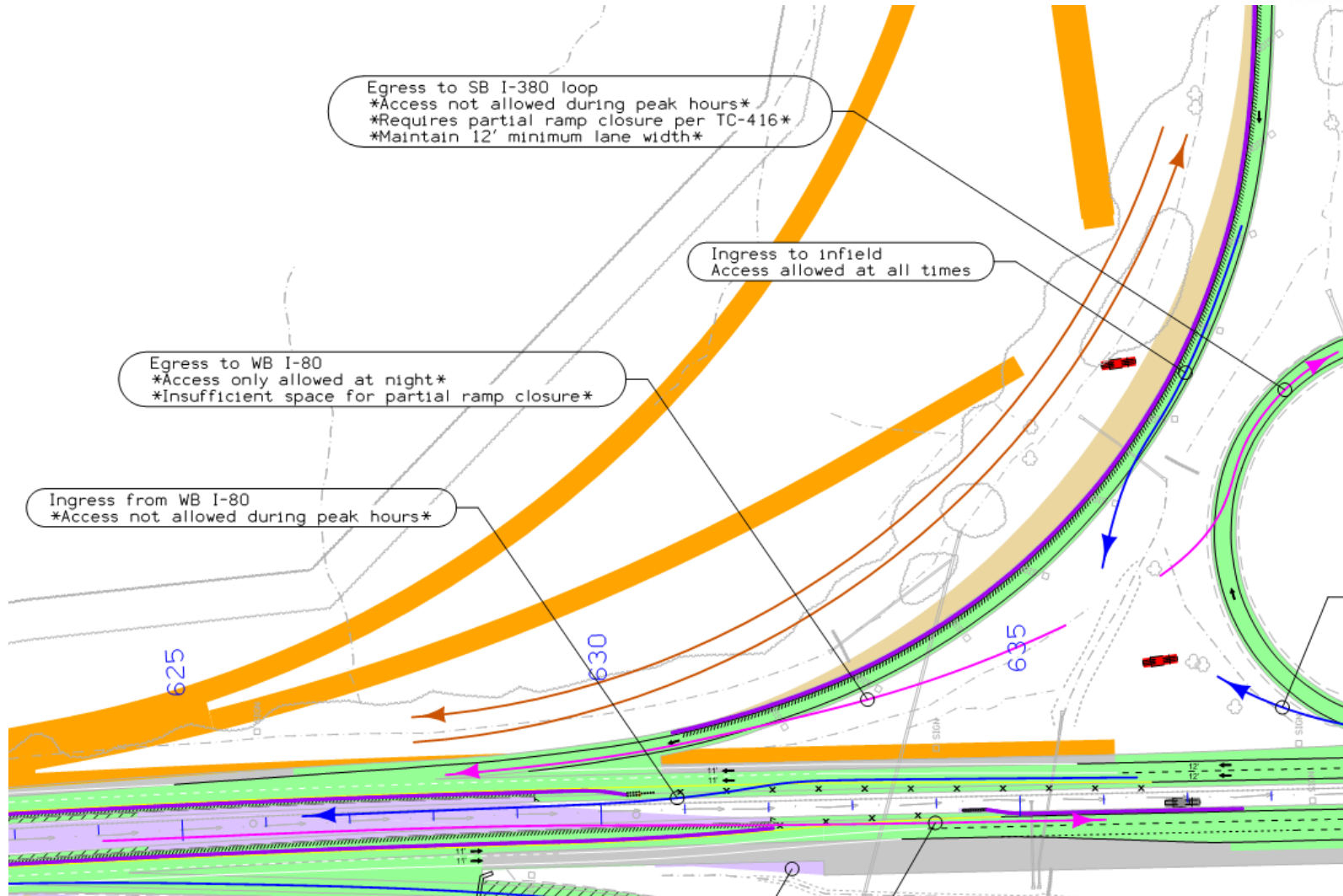
STAGING – INGRESS / EGRESS



STAGING – INGRESS / EGRESS



STAGING – INGRESS / EGRESS



BARRIER RAIL - FINAL

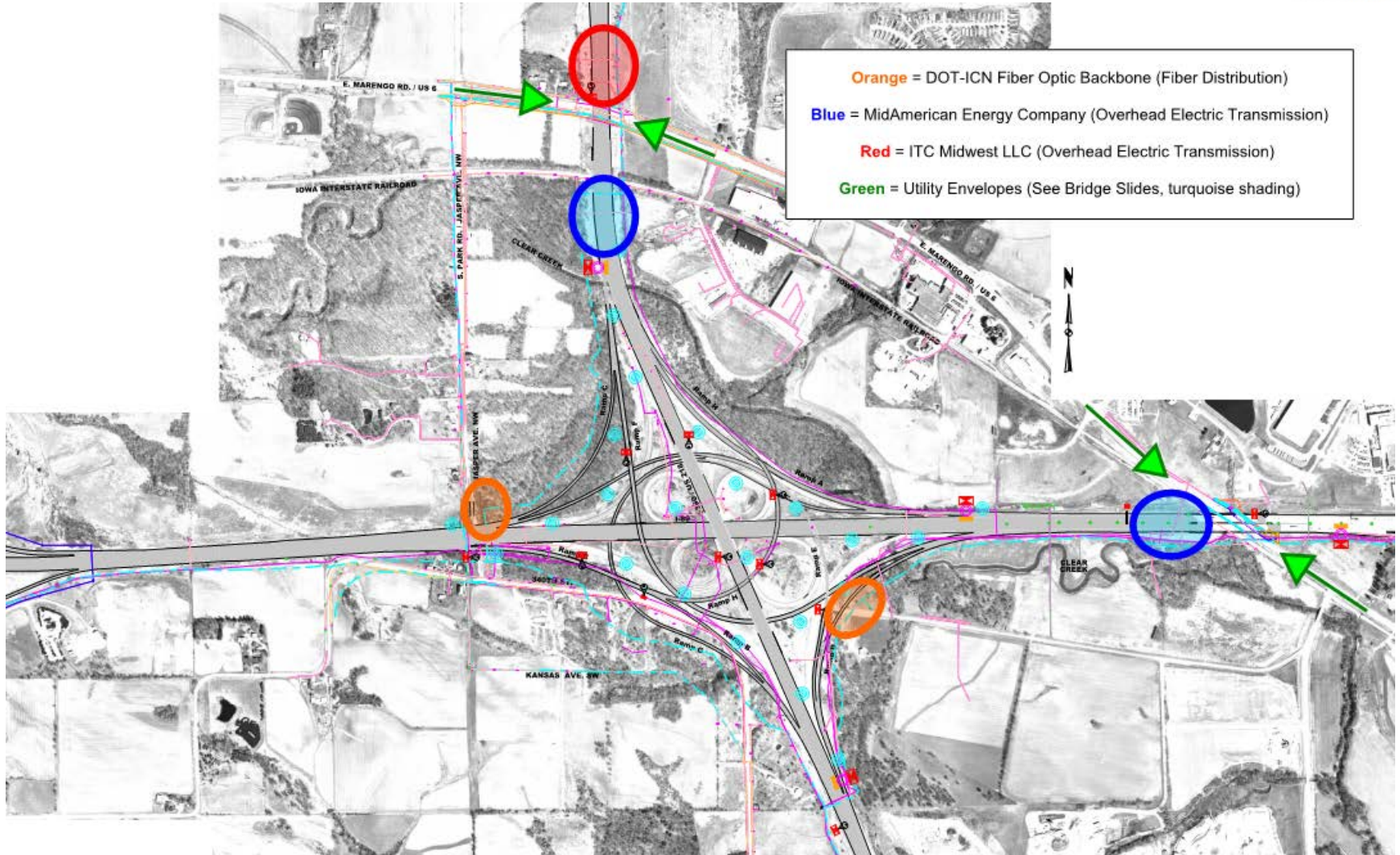


- » I-80 Median Barrier
 - » Aesthetics
 - » Matches I-80 to the East
- » I-380
 - » Same shape
 - » No aesthetic treatments
 - » No texture
 - » No color

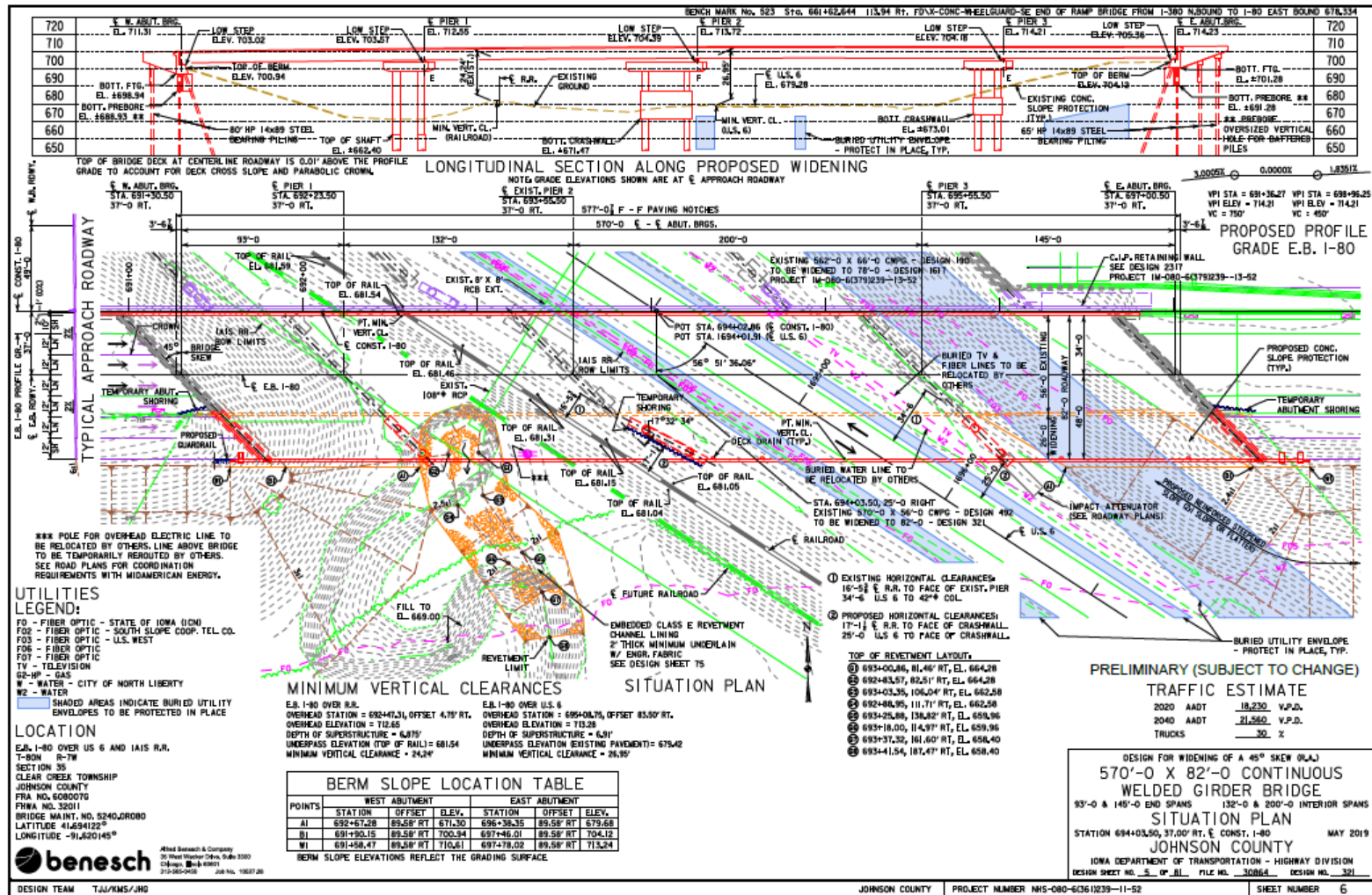


Matt McLaughlin

UTILITIES



EXAMPLE OF UTILITY RELOCATION ENVELOPES



BREAK!



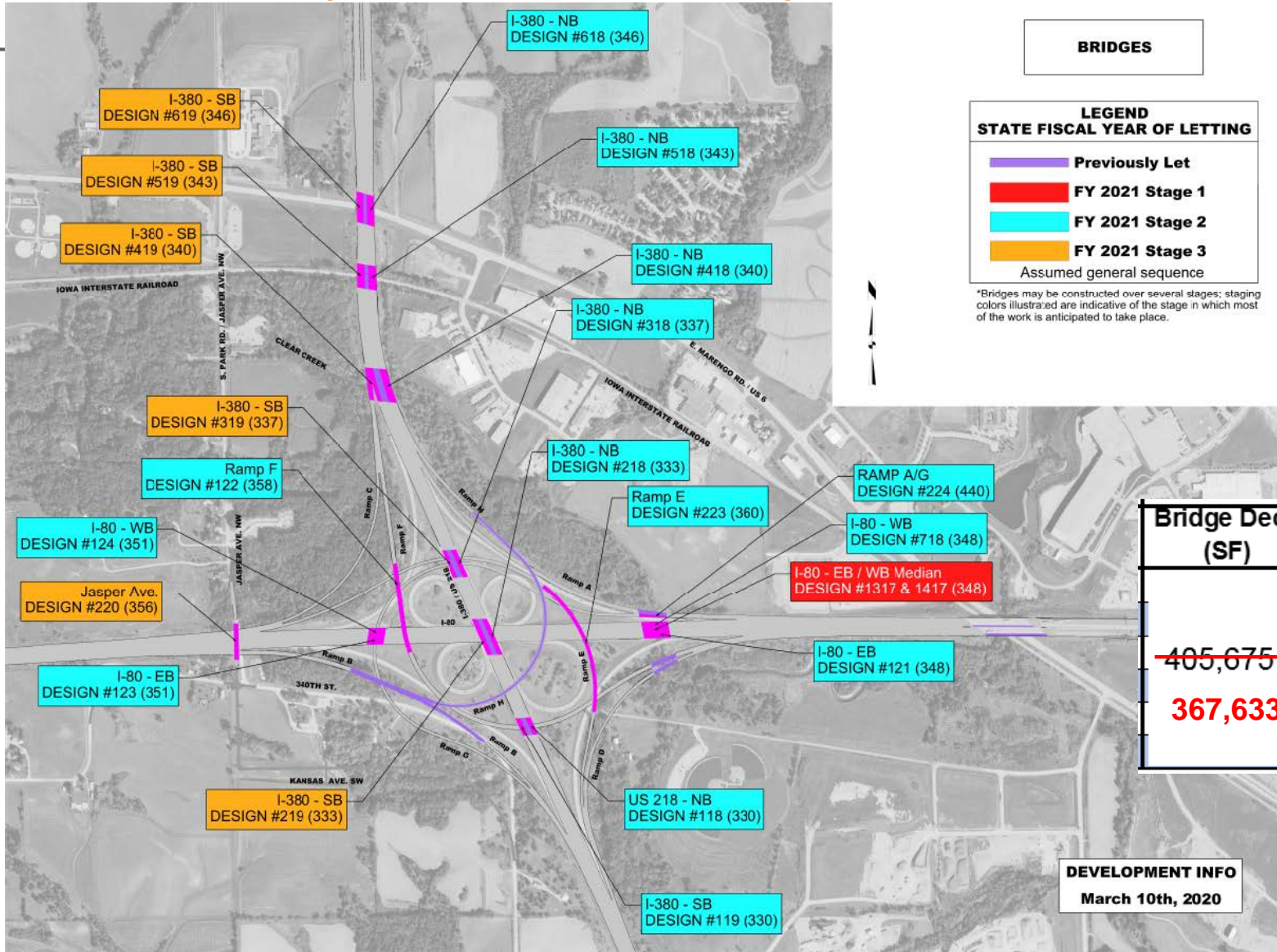
BRIDGES AND STRUCTURES



Steve Maifield

BRIDGES (REFER TO HANDOUT)

9-Apr-20 Bridge Deck SF estimate correction



BRIDGES

LEGEND
STATE FISCAL YEAR OF LETTING

- Previously Let
- FY 2021 Stage 1
- FY 2021 Stage 2
- FY 2021 Stage 3

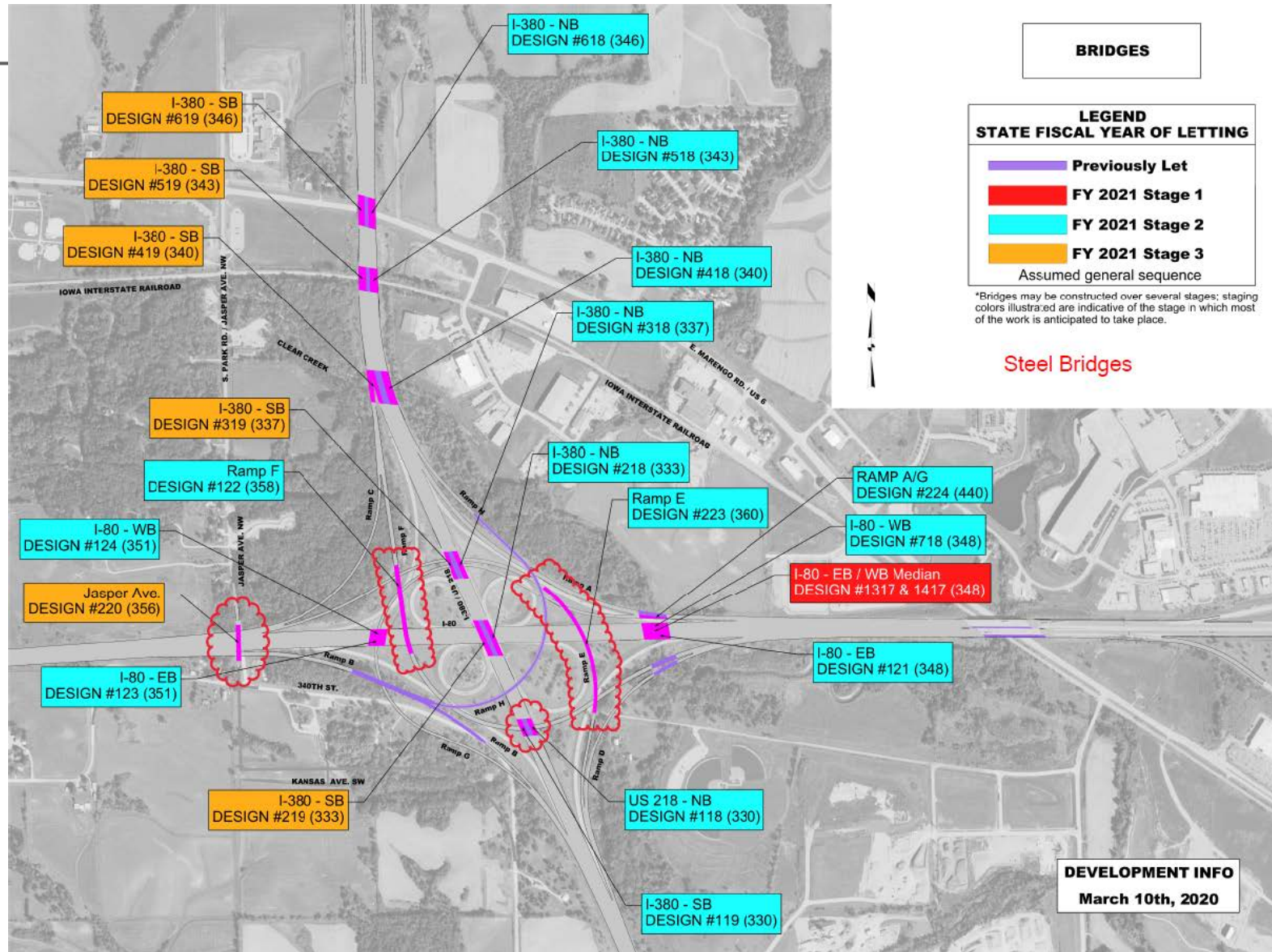
Assumed general sequence

*Bridges may be constructed over several stages; staging colors illustrated are indicative of the stage in which most of the work is anticipated to take place.

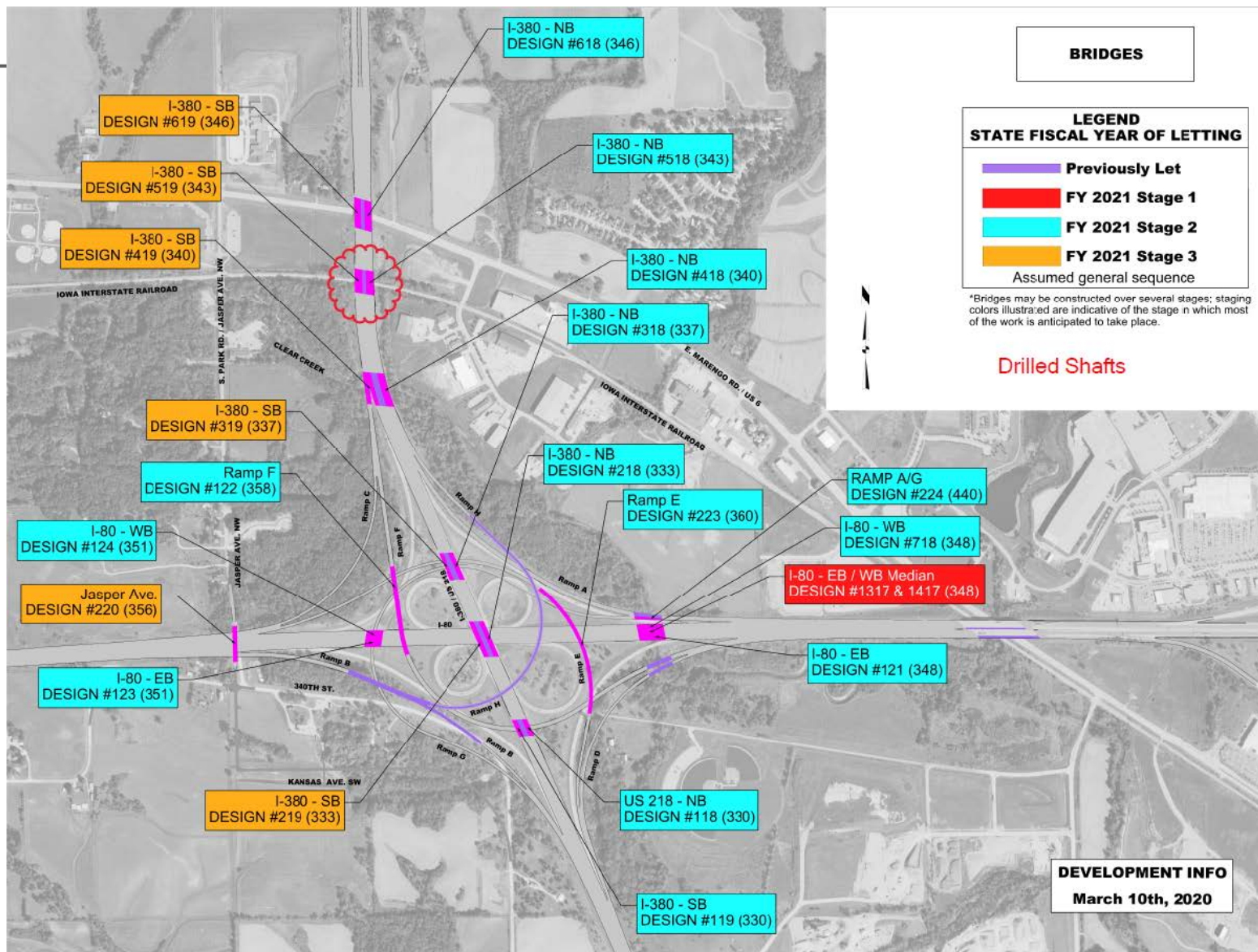
Bridge Deck (SF)
405,675
367,633

DEVELOPMENT INFO
 March 10th, 2020

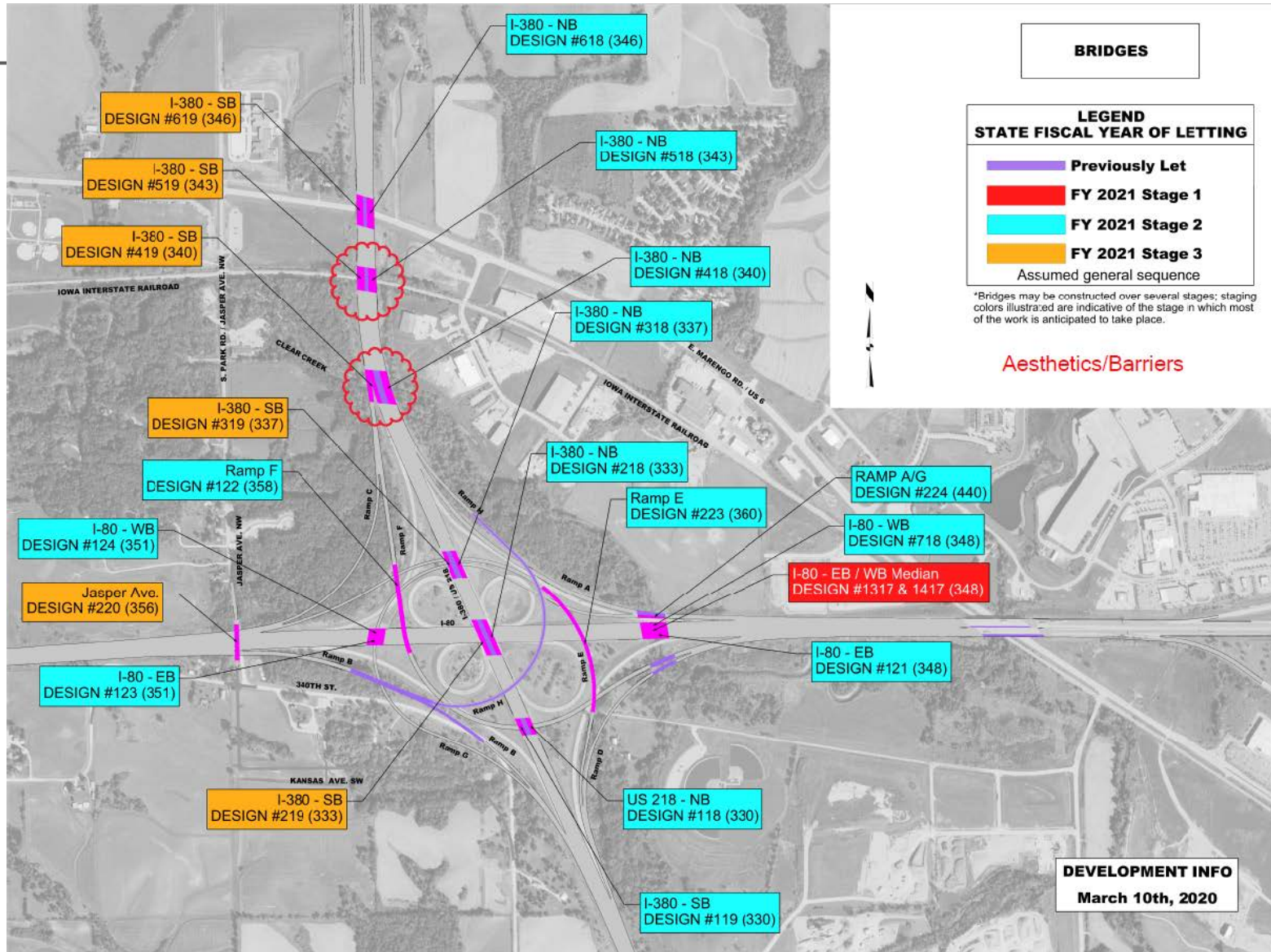
BRIDGES – STEEL



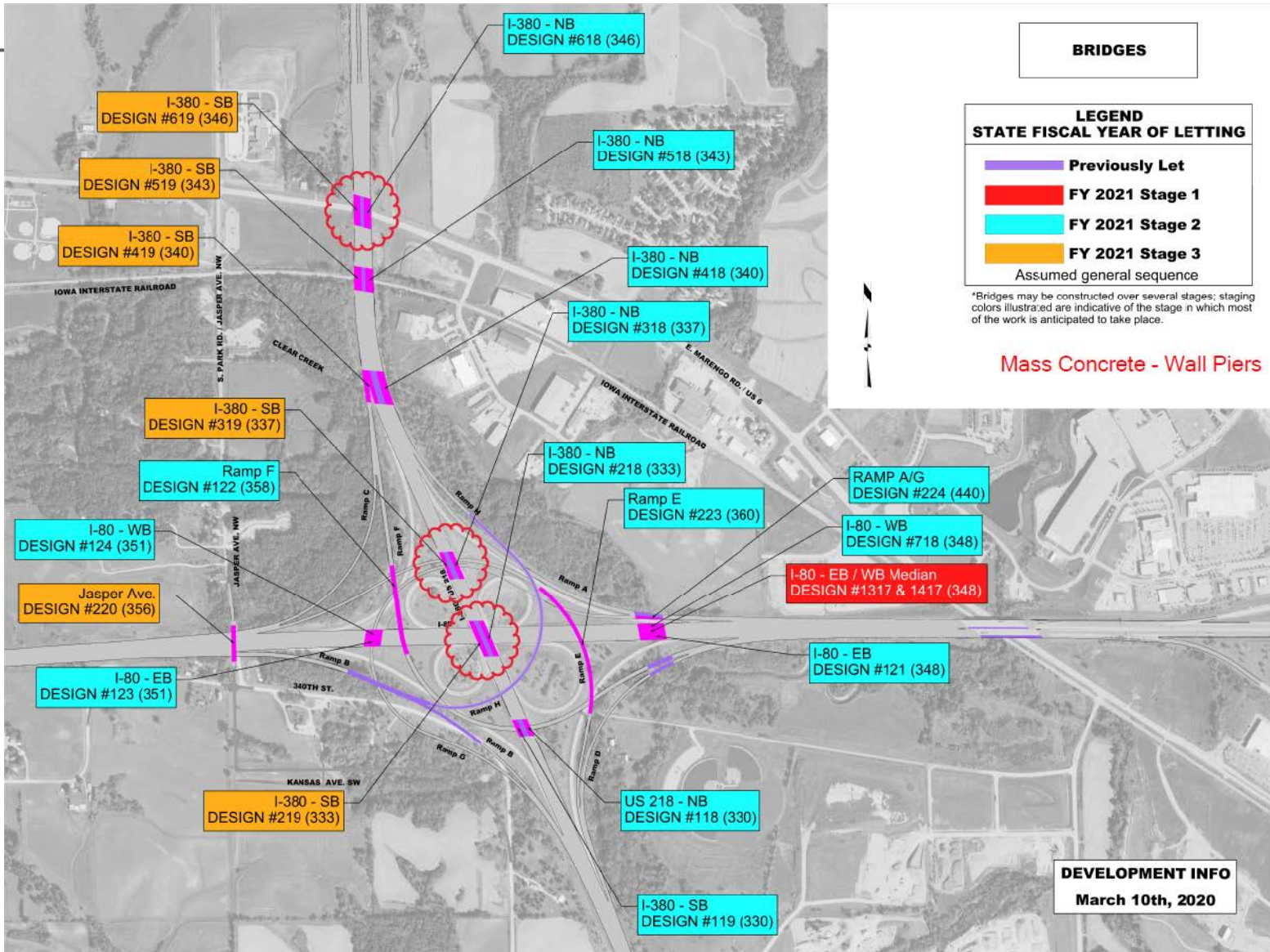
BRIDGES – DRILLED SHAFT



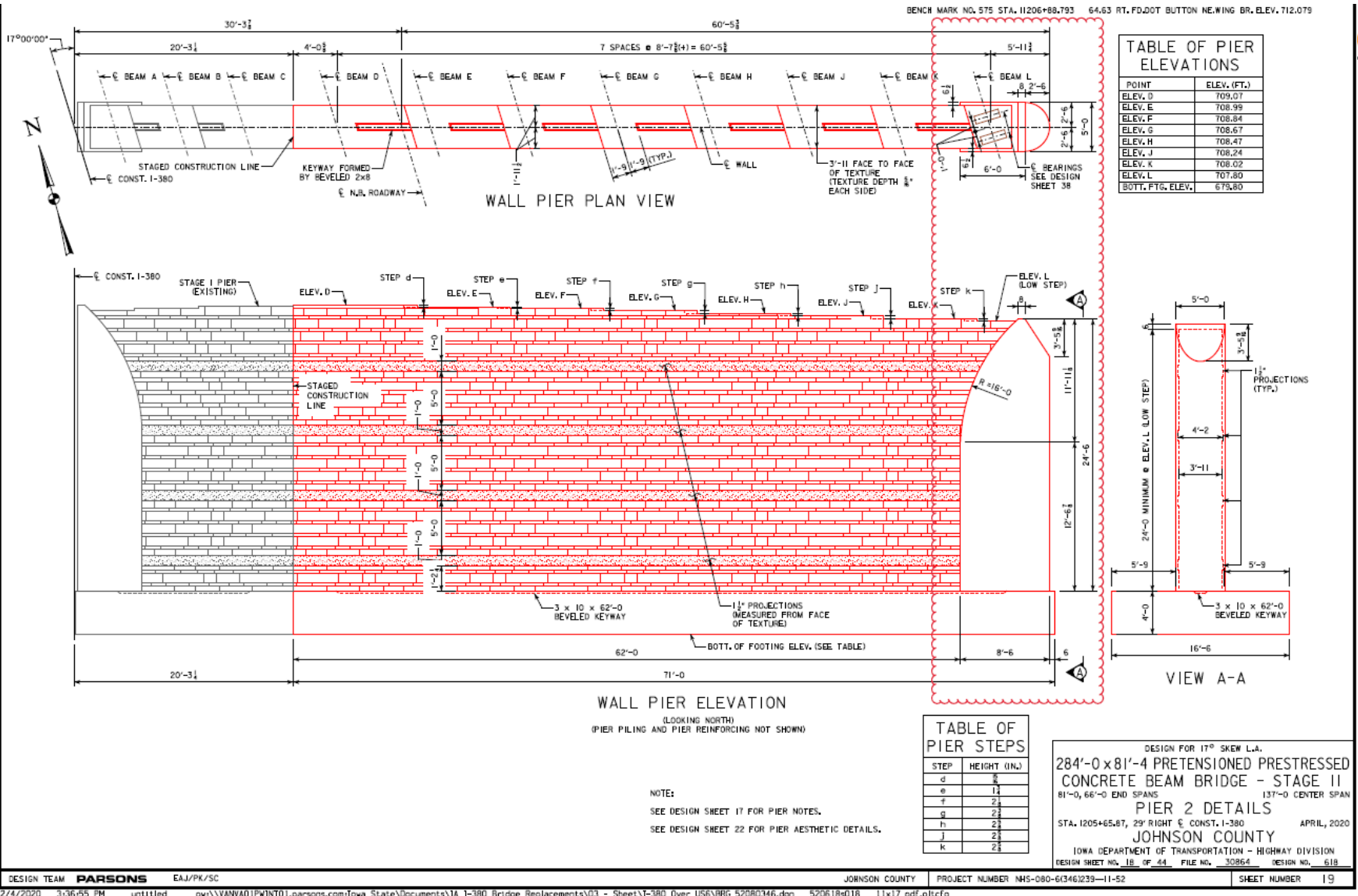
BRIDGES – AESTHETICS



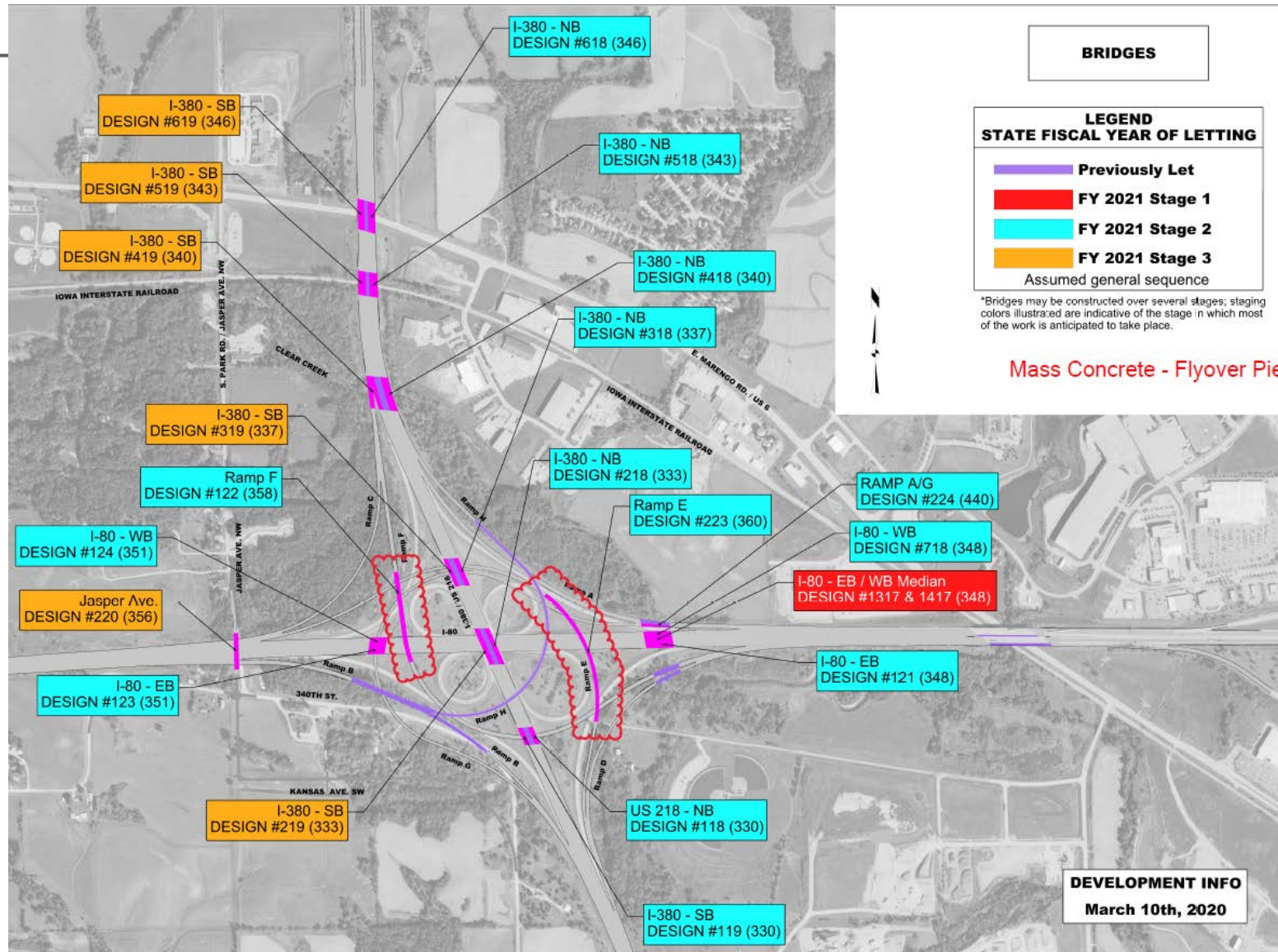
BRIDGES – MASS CONCRETE – WALL PIERS



BRIDGES – MASS CONCRETE – WALL PIERS

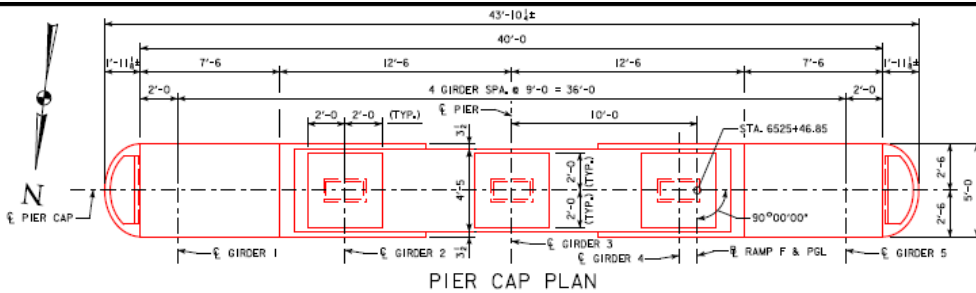


BRIDGES – MASS CONCRETE – FLYOVER PIERS

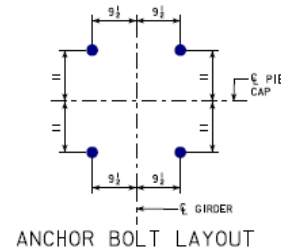


BRIDGES – MASS CONCRETE – FLYOVER PIERS

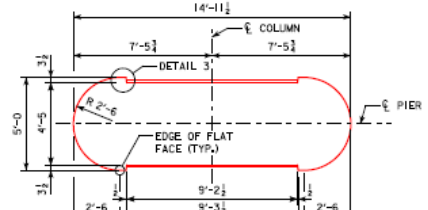
BENCH MARK No. 519 STA. 1165+48.102 77.23 LF. FLYOVER-BW-NW-WING-S-BOUND I-380 BRIDGE OVER I-80 737.970



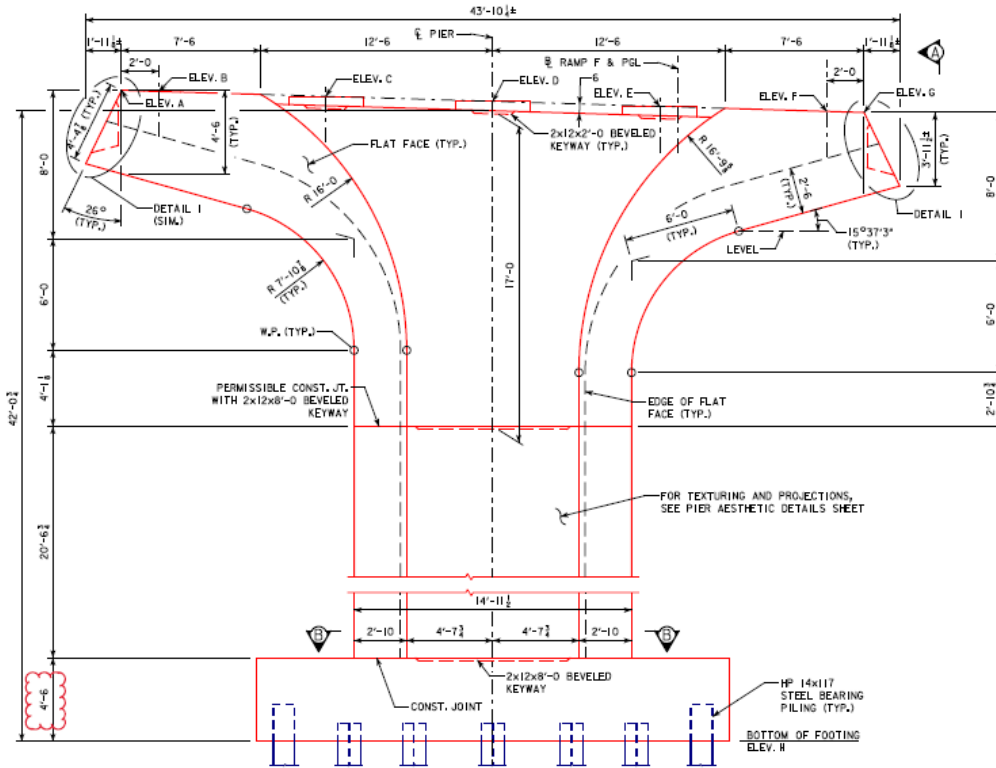
PIER CAP PLAN



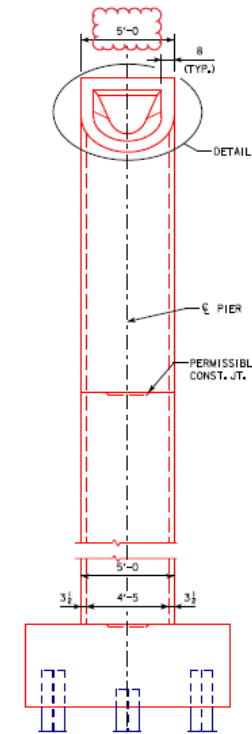
ANCHOR BOLT LAYOUT



SECTION B-B



PIER ELEVATION



VIEW A-A

PIER ELEVATIONS		
LOCATION	PLAN	AS-BUILT
ELEV. A	739.91	
ELEV. B ▲	739.85	
ELEV. C	739.88	
ELEV. D	739.31	
ELEV. E	739.04	
ELEV. F ▲	738.77*	
ELEV. G	738.71	
ELEV. H	696.75	

* LOW STEP
▲ SEE TRANSVERSE SLOPE OF MASONRY PLATE ON DESIGN SHEET 45 FOR SLOPE OF BEARING AREA.

- NOTES:**
- FOR PIER REINFORCEMENT, SEE DESIGN SHEETS 24 THROUGH 22.
 - FOR FOOTING DETAILS, SEE DESIGN SHEET 24.
 - FOR REINFORCING BAR LIST, SEE DESIGN SHEET 28.
 - FOR PIER AESTHETIC DETAILS, SEE DESIGN SHEET 41.
 - FOR CONCRETE PAINTING DETAILS, SEE DESIGN SHEET 42.
 - FOR DETAILS 1, 2 & 3, CAP END REINFORCEMENT AND PIER NOTES, SEE DESIGN SHEET 22.

benesch Allied Benesch & Company
35 West Wacker Drive, Suite 3300
Chicago, IL 60601
312.565.4000 Fax 312.565.4002

DESIGN TEAM AJ5/KMS/SCD

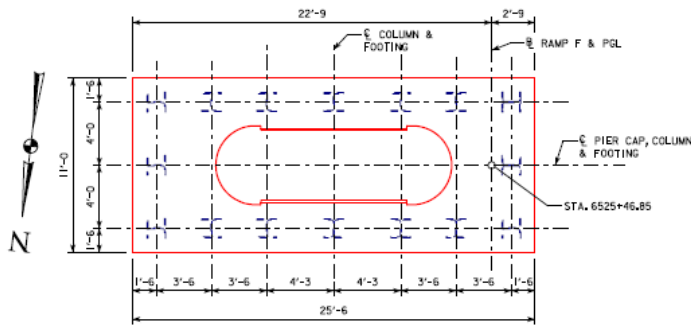
11:02:54 AM untitled c:\pwordir\benesch_projects\projects\dms39575\BRG_52080358.dgn

DESIGN FOR 10°50'21" SKEW
854'-0 X 40'-0 CONTINUOUS
WELDED GIRDER BRIDGE
160'-10, 170'-0, 188'-0, 170'-0, 165'-11 SPANS
PIER I DETAILS
STA. 6528+13.00 (RAMP F) APRIL 2020
JOHNSON COUNTY
IOWA DEPARTMENT OF TRANSPORTATION - HIGHWAY DIVISION
DESIGN SHEET NO. 23 OF 85 FILE NO. 30864 DESIGN NO. 122

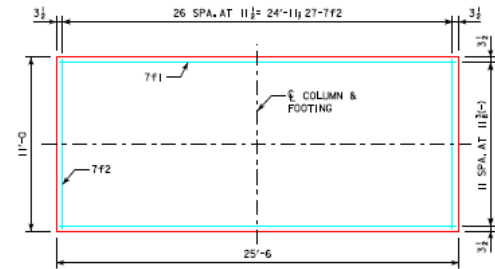
JOHNSON COUNTY PROJECT NUMBER IM-080-6(235)239-11-52

SHEET NUMBER 24

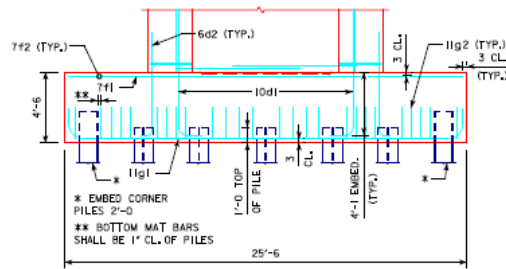
BRIDGES – MASS CONCRETE – FLYOVER PIERS



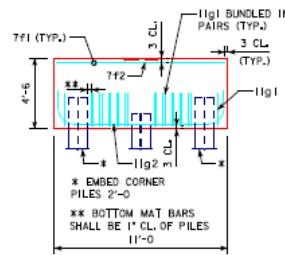
PILE LAYOUT



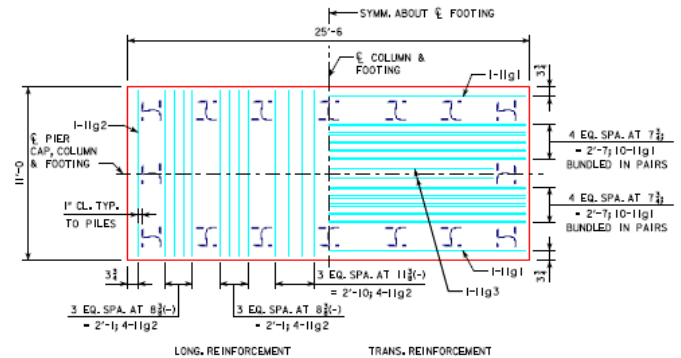
TOP FOOTING REINFORCING LAYOUT



FOOTING ELEVATION



FOOTING SIDE ELEVATION



BOTTOM FOOTING REINFORCING LAYOUT

PIER 1 PILING NOTES:

THE CONTRACT LENGTH OF 75 FEET FOR THE PIER 1 PILES IS BASED ON A COHESIVE SOIL CLASSIFICATION, A TOTAL FACTORED AXIAL LOAD PER PILE (PL) OF 425 KIPS, AND A GEOTECHNICAL RESISTANCE FACTOR (PHI) OF 0.7 FOR ROCK END BEARING. PILES WERE ALSO DESIGNED FOR A FACTORED TENSION FORCE OF 23 KIPS.

THE NOMINAL AXIAL BEARING RESISTANCE FOR CONSTRUCTION CONTROL WAS DETERMINED FROM A COHESIVE SOIL CLASSIFICATION AND A GEOTECHNICAL RESISTANCE FACTOR (PHI) OF 0.7 FOR ROCK END BEARING. PILES ARE ASSUMED TO BE DRIVEN FROM A START ELEVATION AT THE BOTTOM OF FOOTING.

THE REQUIRED NOMINAL AXIAL BEARING RESISTANCE FOR THE PIER 1 PILES IS 304 TONS AT END OF DRIVE OR RETAP. THE PILE CONTRACT LENGTH SHALL BE DRIVEN AS PER PLAN UNLESS PILES REACH REFUSAL. IN NO CASE SHALL A PILE BE EMBEDDED LESS THAN 25 FEET. CONSTRUCTION CONTROL REQUIRES A WEAP ANALYSIS WITH BEARING GRAPH.

PIER PILES ARE DESIGNED TO ACCOMMODATE DOWNDRAG FORCE DUE TO SOIL CONSOLIDATION UNDER THE NEW EARTH FILL. PILES SHALL BE DRIVEN TO 213 TONS OF RESISTANCE BASED ON THEORETICAL DRIVING RESISTANCE THIS INCLUDES 9 TONS RESISTANCE FOR DOWNDRAG FORCES AND 204 TONS RESISTANCE FOR DEAD AND LIVE LOAD BEARING CAPACITY.

PILE DIMENSIONS ARE AT BOTTOM OF FOOTING.

16 - HP14x117 STEEL BEARING PILES ARE REQUIRED.

CAST IN-ONE-PIECE STEEL PILE POINTS ARE REQUIRED FOR THE PIER PILES IN ACCORDANCE WITH ARTICLE 4167.02 OF THE CURRENT STANDARD SPECIFICATIONS AND MATERIALS 1M 46B.

BUNDLED BARS SHALL BE TIED, WIRED, OR OTHERWISE FASTENED TO ENSURE THAT THEY REMAIN IN THEIR RELATIVE POSITION.

NOTES:

FOR PIER REINFORCING, SEE DESIGN SHEETS 24 THROUGH 27 AND 22.

FOR REINFORCING BAR LIST, SEE DESIGN SHEET 28.

DESIGN FOR 10°50'21" SKEW
 854'-0" X 40'-0" CONTINUOUS
 WELDED GIRDER BRIDGE
 160'-10", 170'-0", 188'-0", 170'-0", 165'-1 1/2" SPANS
 PIER 1 DETAILS
 STA. 6526+13.00 @ RAMP F) APRIL 2020
 JOHNSON COUNTY
 IOWA DEPARTMENT OF TRANSPORTATION - HIGHWAY DIVISION
 DESIGN SHEET NO. 24 OF 85 FILE NO. 30864 DESIGN NO. 122



DESIGN TEAM AJS/RMS/SCD

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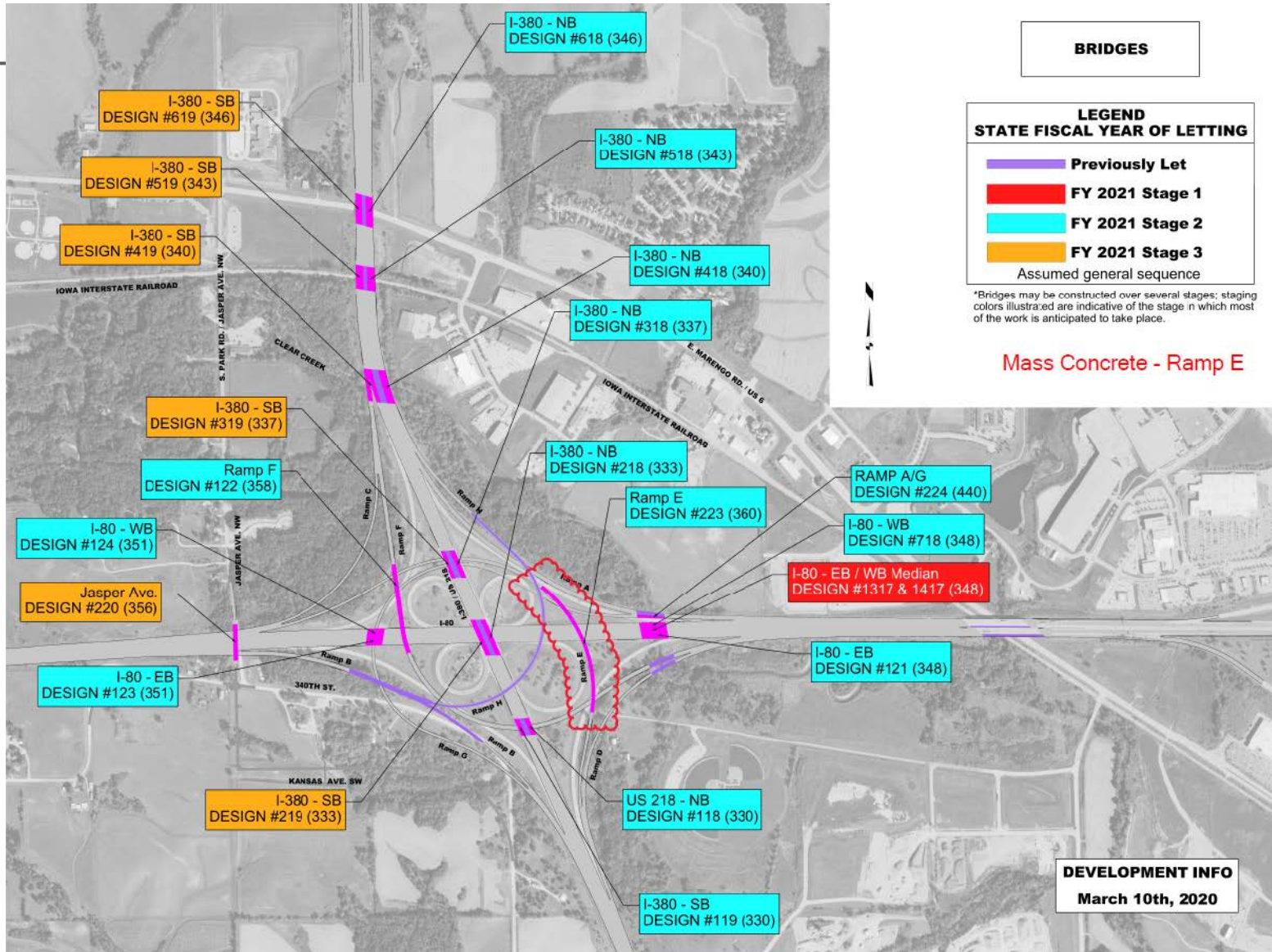
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JOHNSON COUNTY

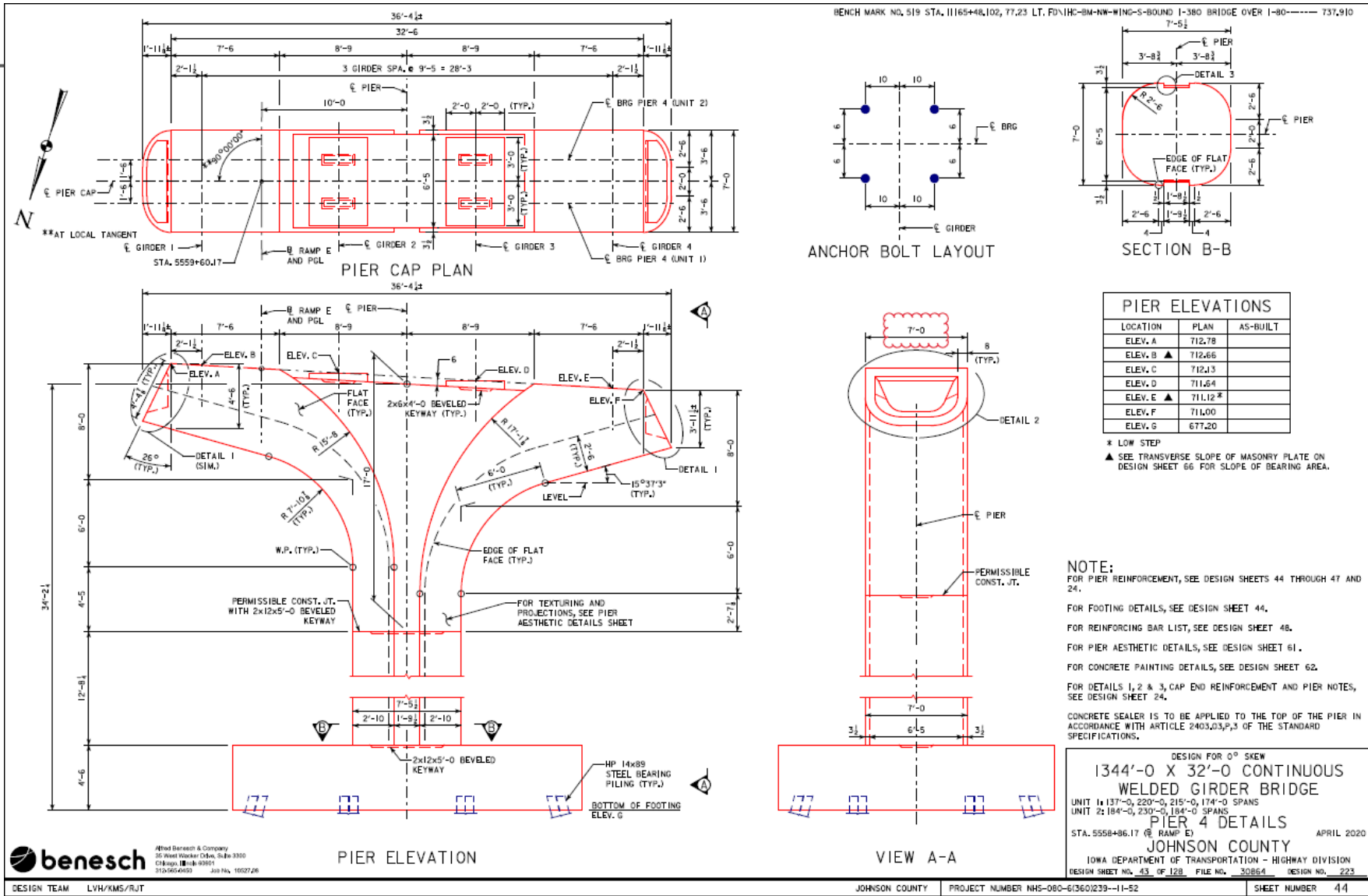
PROJECT NUMBER IM-080-6(358)239-11-52

SHEET NUMBER 25

BRIDGES – MASS CONCRETE – RAMP E



BRIDGES – MASS CONCRETE – RAMP E



benesch
Atwood Benesch & Company
35 West Wacker Drive, Suite 3300
Chicago, IL 60601
312.940.4900 Fax No. 312.940.2126

DESIGN TEAM LVH/XMS/RJT
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JOHNSON COUNTY PROJECT NUMBER NHS-080-6(360)239--11-52 SHEET NUMBER 44

CONTRACT INFORMATION



Stacy Ryan

CONTRACT – FY21



- » Advertisement May 19, 2020
- » Special Letting July 15, 2020
- » Site Milestones and Completion
- » Bonuses
- » Special Provisions

FY21 CONSTRUCTION COMMITMENTS AND SCHEDULE



- » July 2020 Letting
- » Substantial Completion Fall 2022 including:
 - » New Interstate lanes and outside shoulders open
 - » New Ramps open
- » Work allowed to be completed in 2023
 - » Listed on slide for site 00 at end of sites
- » Contract completed Summer 2023

CONTRACT OVERVIEW



Draft Contract Time

Proposal ID- 52-0806-372

Letting: July 15, 2020

Site ID	Site Details	Liquidated Damages
00	Completion Date Overall Project	7/15/2023 \$4,000
01	Completion Date Complete I-80 WB to I-380 NB Ramp A	11/18/2022 \$14,500
02	Completion Date Complete I-380 SB to I-80EB Ramp F	11/18/2022 \$14,500
03	Completion Date Complete I-80 WB	11/18/2022 \$10,000
04	Completion Date Complete I-80 EB	11/18/2022 \$10,000
05	Completion Date Complete US-218 NB to I-80 WB Ramp E	11/18/2022 \$6,000
06	Completion Date Complete I-80 WB to US-218 SB Ramp G	11/18/2022 \$6,000
07	Completion Date Substantial Completion of Interchanges. See details below.	11/18/2022 \$10,000

CONTRACT OVERVIEW



Draft Contract Time

Proposal ID- 52-0806-372

Letting: July 15, 2020

Site ID	Site Details	Liquidated Damages	
08	No Specified Start Date Critical Closure Ramp H.	10 Calendar Days	\$10,500
09	No Specified Start Date Critical Closure Ramp H / B.	5 Calendar Days	\$22,000
10	No Specified Start Date Critical Closure Ireland Ramp D.	90 Calendar Days	\$10,000
11	No Specified Start Date Critical Closure Ramp B.	5 Calendar Days	\$11,500
12	No Specified Start Date Critical Closure IA 965 Ramp C.	14 Calendar Days	\$19,500
13	No Specified Start Date Critical Closure Ramp C.	14 Calendar Days	\$20,000
14	No Specified Start Date Critical Closure Ramp D.	90 Calendar Days	\$17,500

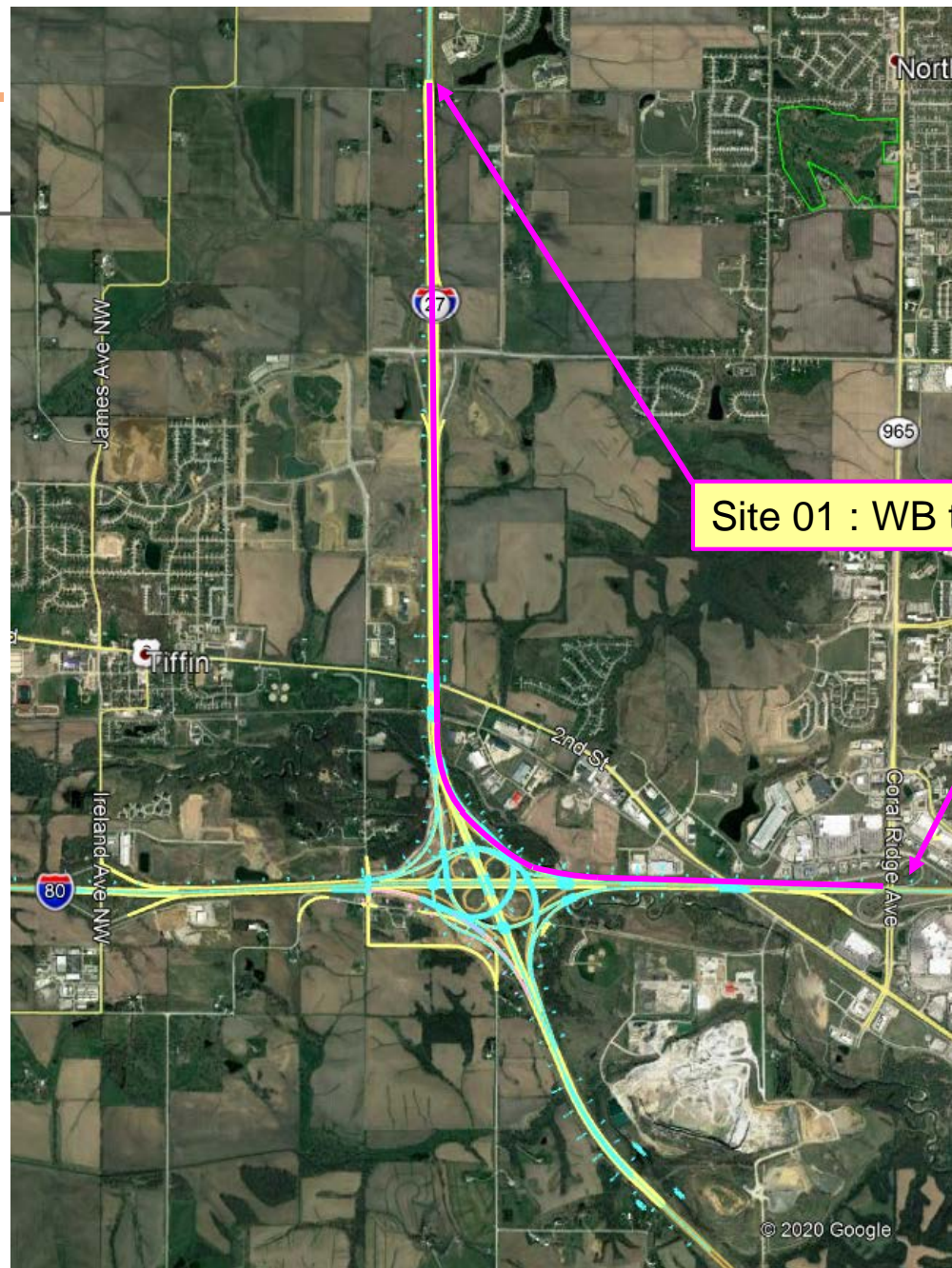
CONTRACT OVERVIEW



Draft No Excuse Bonuses

	Bonus	Date	Amount
No.1	Complete I-380 SB Stage 2F Traffic Switch	11/17/2021	\$350,000
No.2	Complete I-80 WB to I-80/I-380 Ramp A to I-380 NB	11/18/2020	\$2,175,000
No.3	Complete I-380 SB to I-80/I-380 Ramp F to I-80 EB	11/18/2020	\$2,175,000
No.4	Complete I-80 WB	11/18/2020	\$1,500,000
No.5	Complete I-80 EB	11/18/2020	\$1,500,000

CONTRACT – SITES (REFER TO HANDOUT)

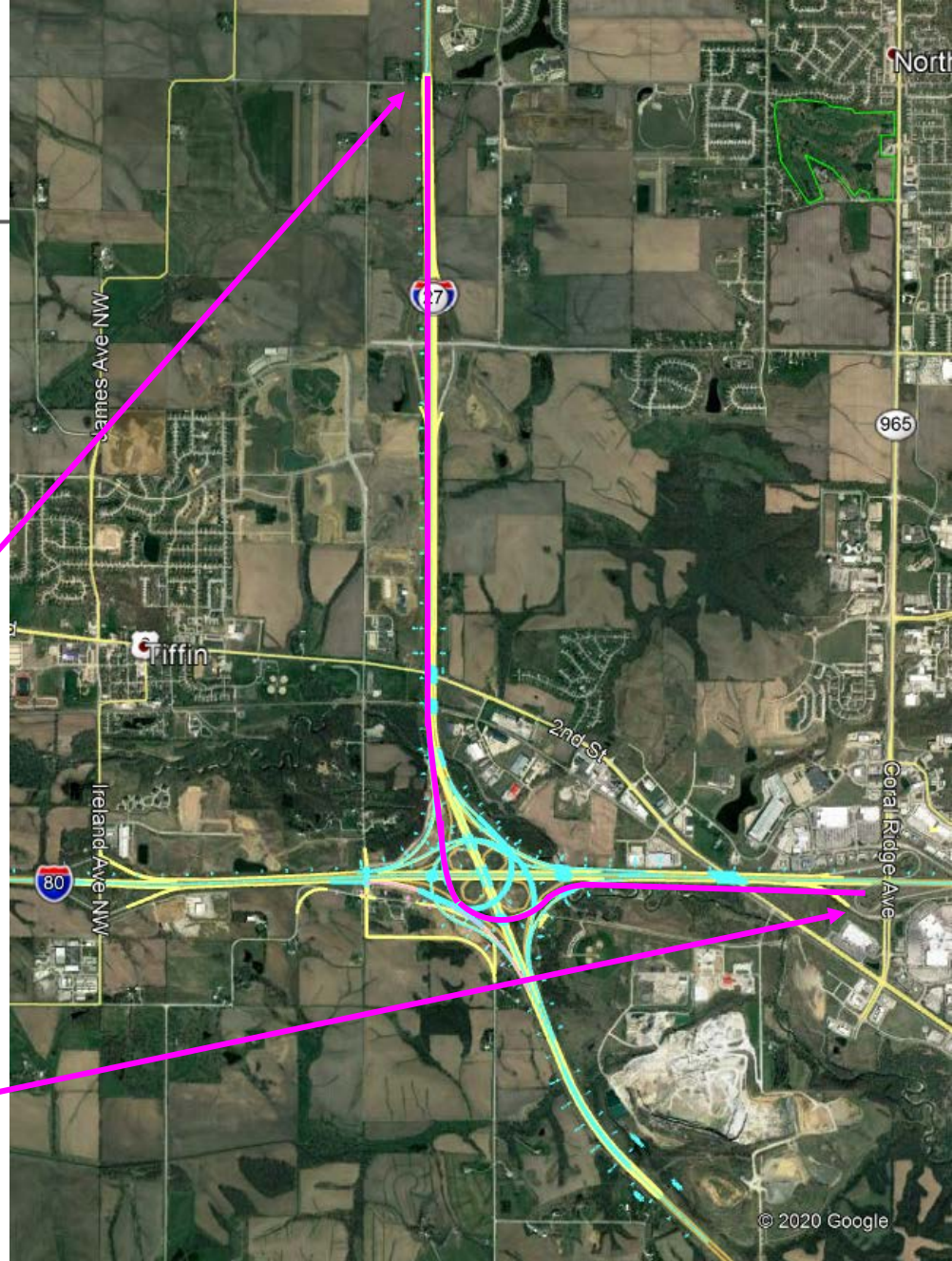


Site 01 : WB to NB

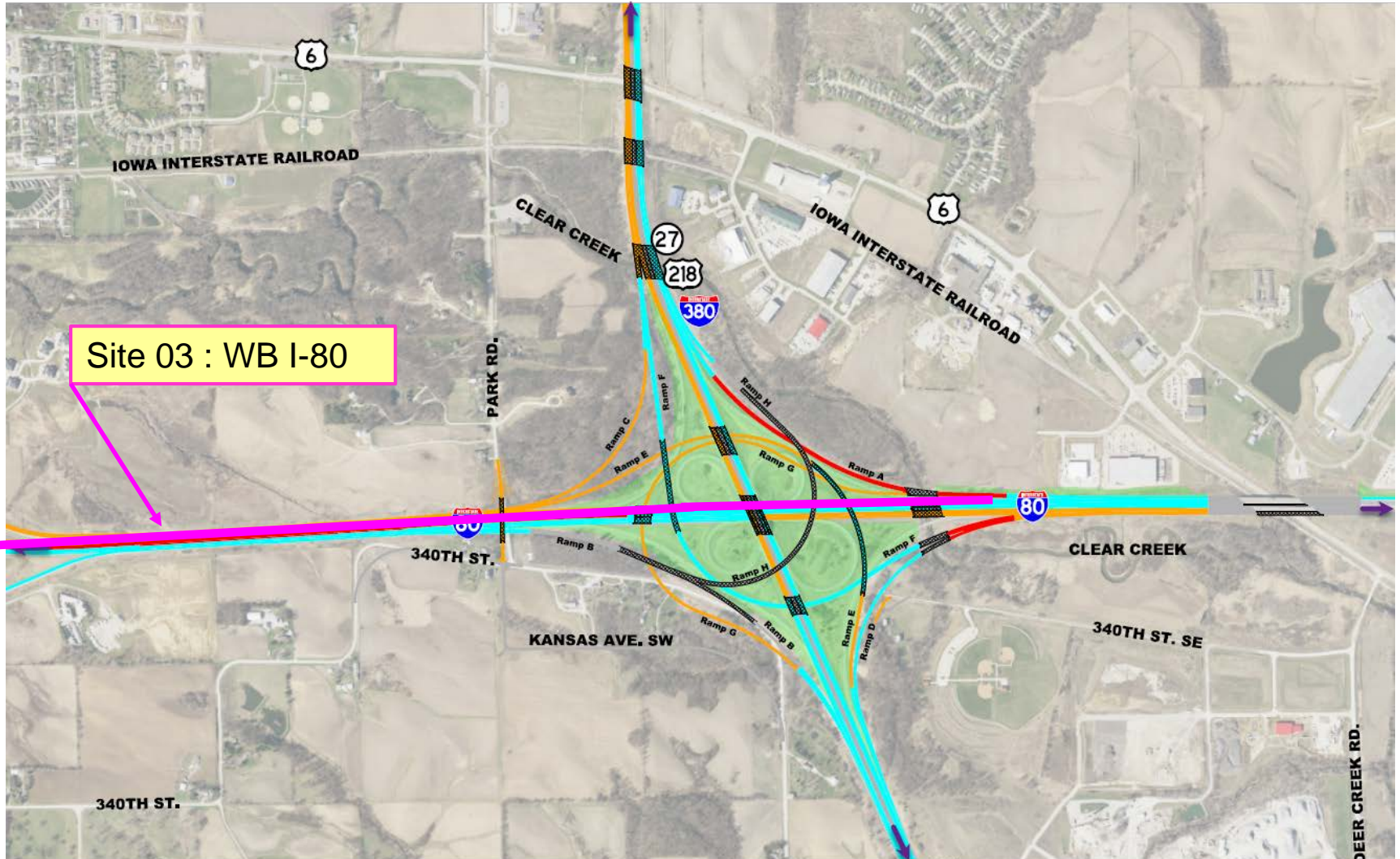
CONTRACT – SITES (REFER TO HANDOUT)



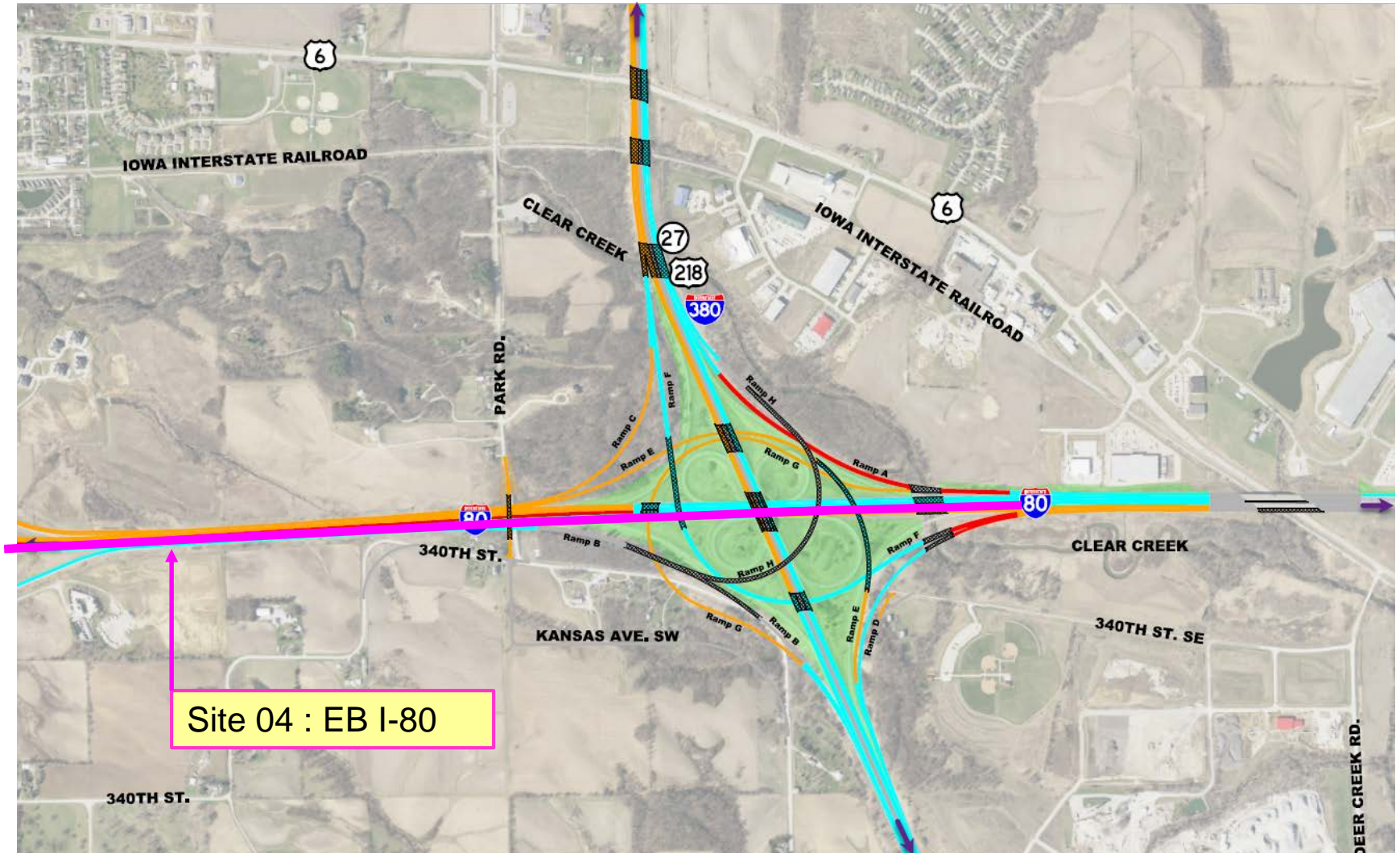
Site 02 : SB to EB



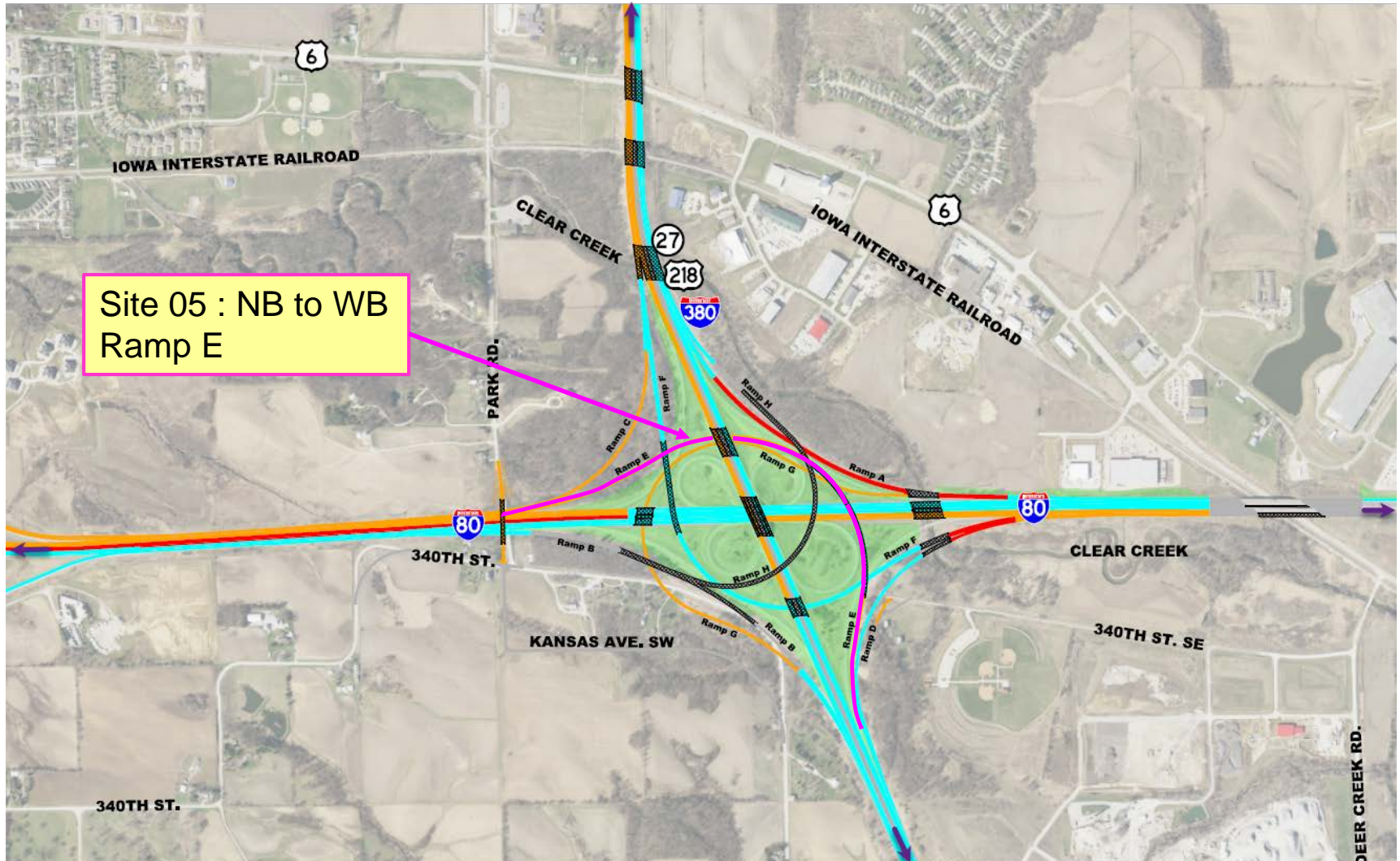
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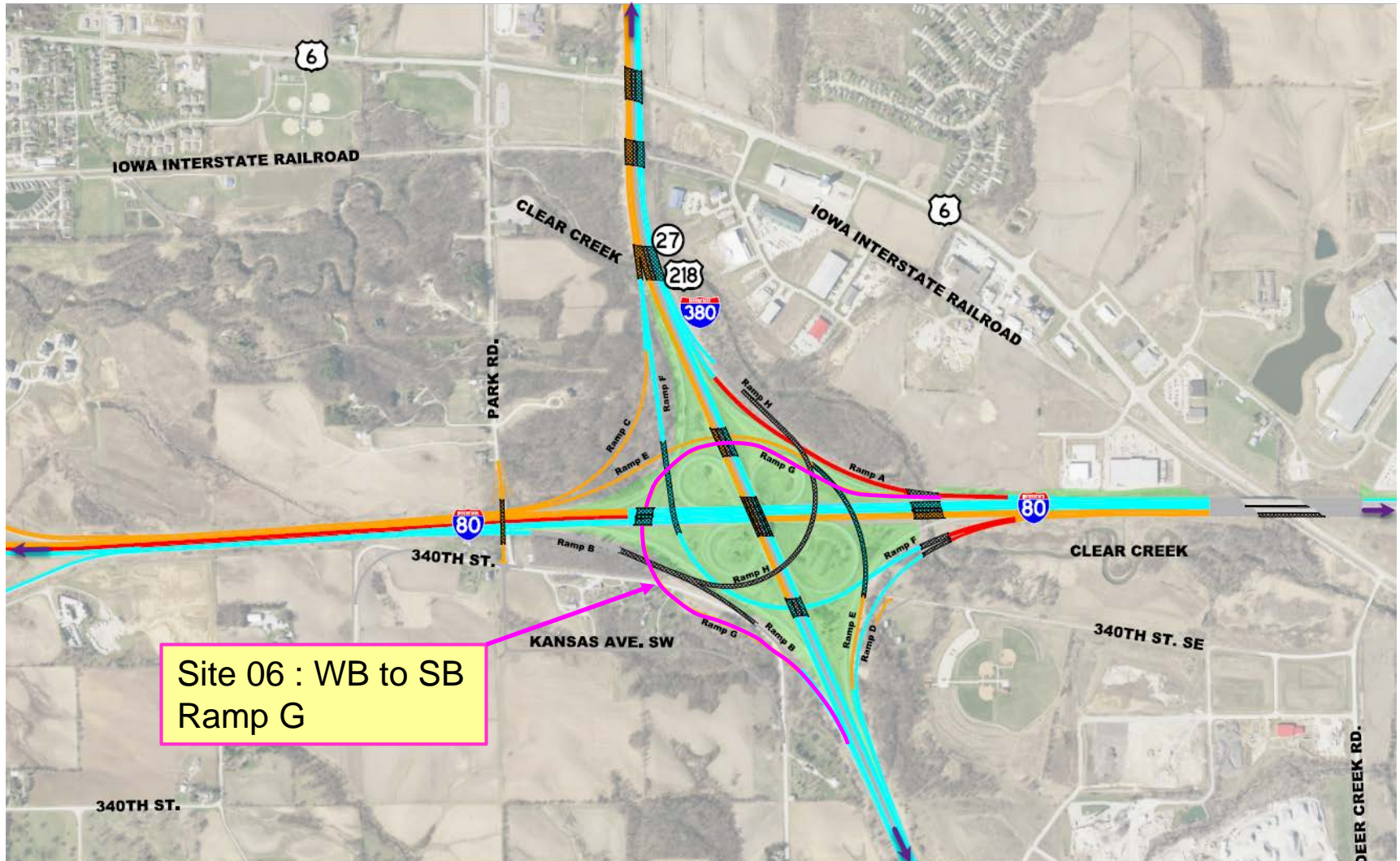
CONTRACT – SITES (REFER TO HANDOUT)



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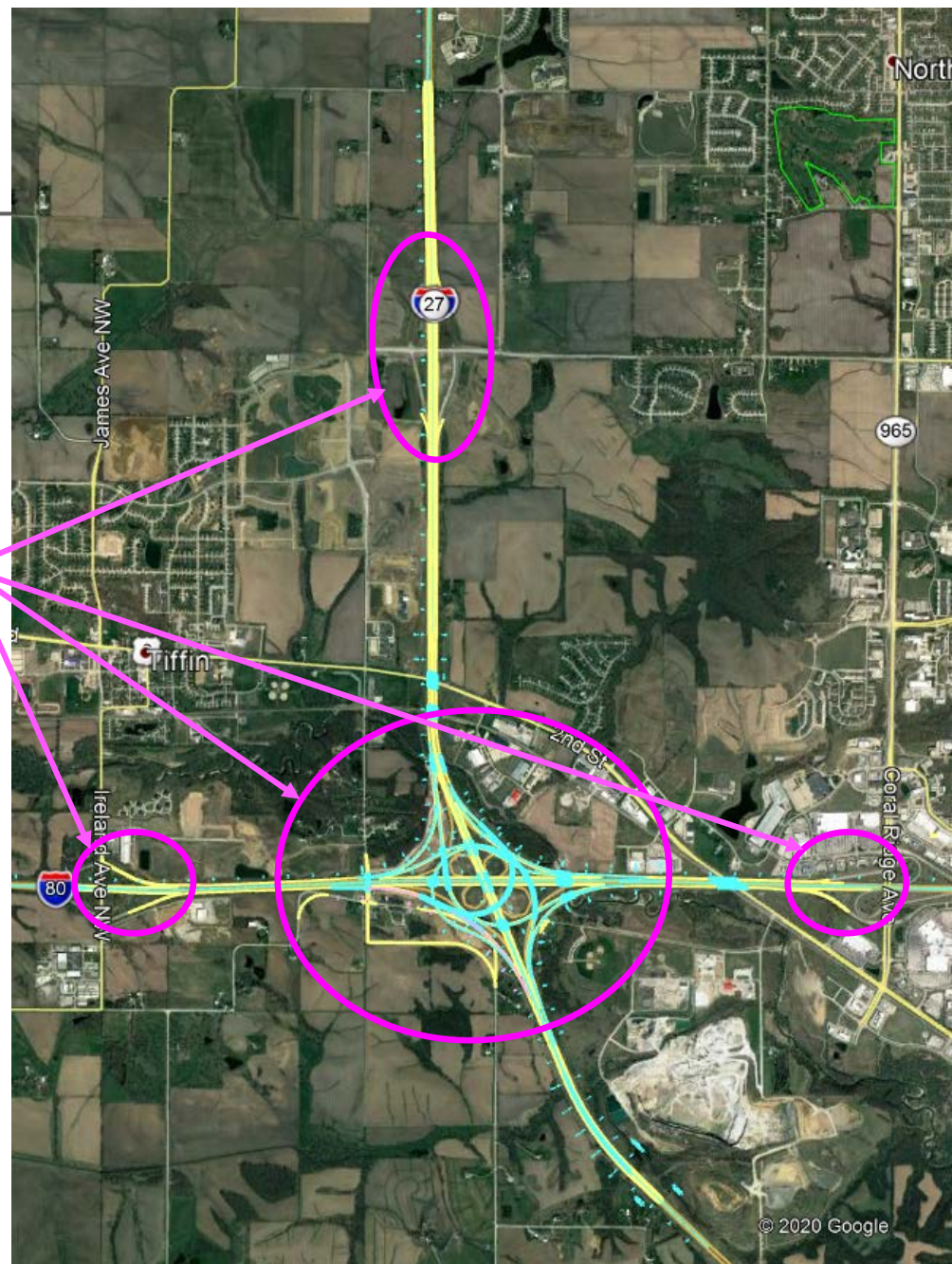


CONTRACT – SITES (REFER TO HANDOUT)

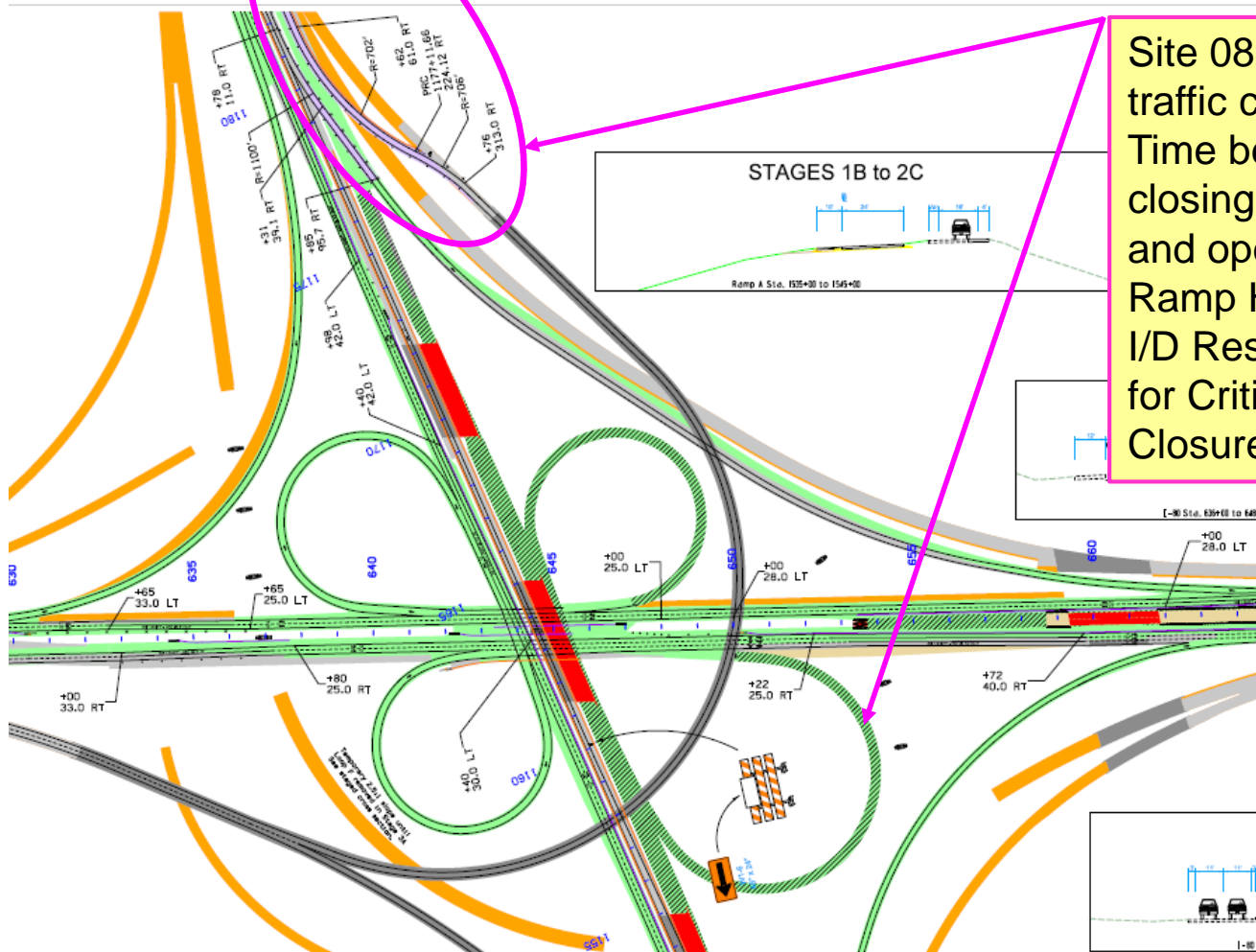


Site 07 : All remaining interchange ramps including:

- 80/380,
- Forevergreen Road,
- Ireland Ave,
- Coral Ridge Ave



CONTRACT – SITES (REFER TO HANDOUT)

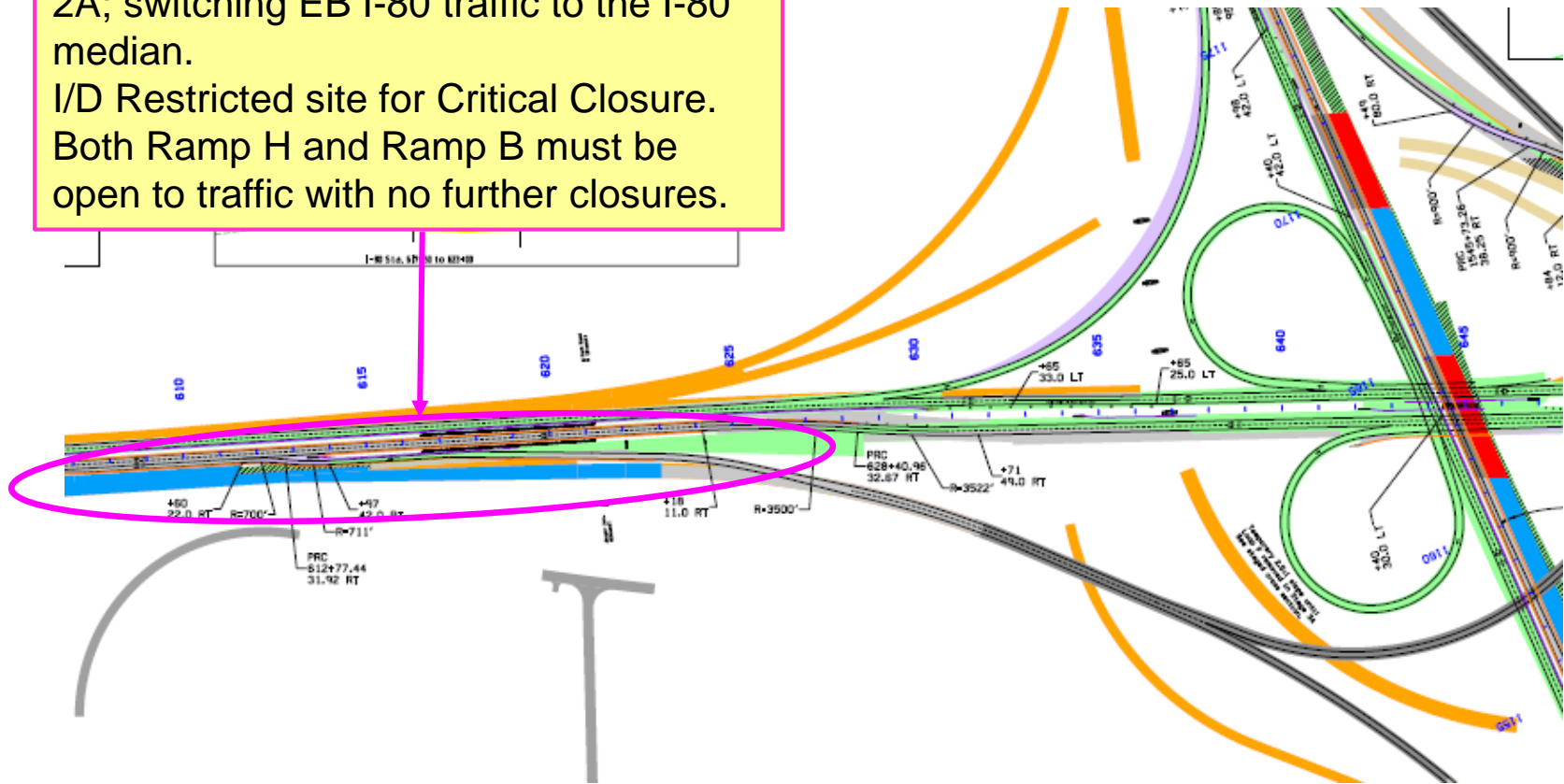


Site 08 : Ramp H traffic closure.
Time between closing the loop and opening new Ramp H to traffic.
I/D Restricted site for Critical Closure.

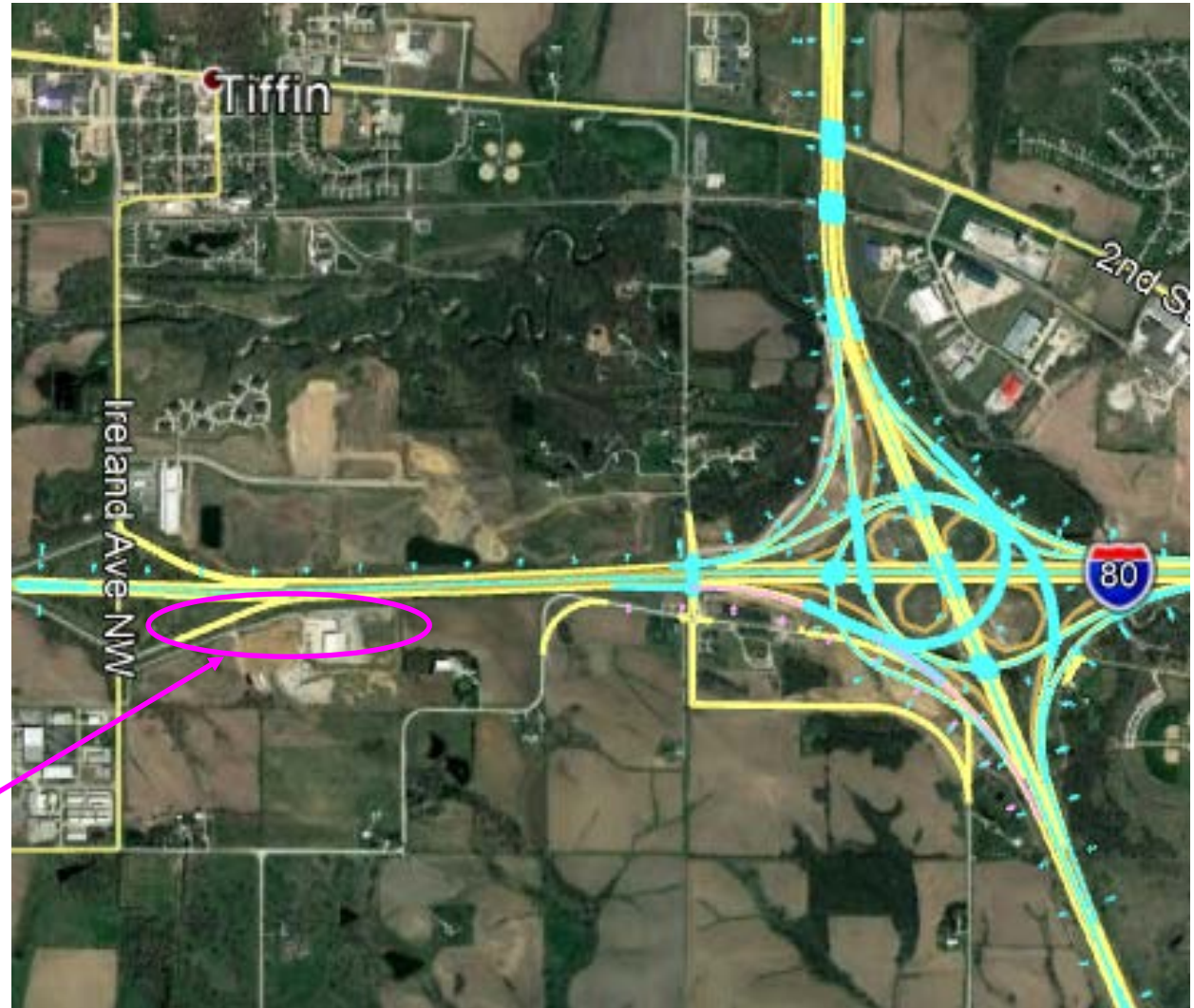
CONTRACT – SITES (REFER TO HANDOUT)



Site 09 : Ramps B and H connections with EB I-80 needed to complete stage 2A; switching EB I-80 traffic to the I-80 median.
I/D Restricted site for Critical Closure.
Both Ramp H and Ramp B must be open to traffic with no further closures.

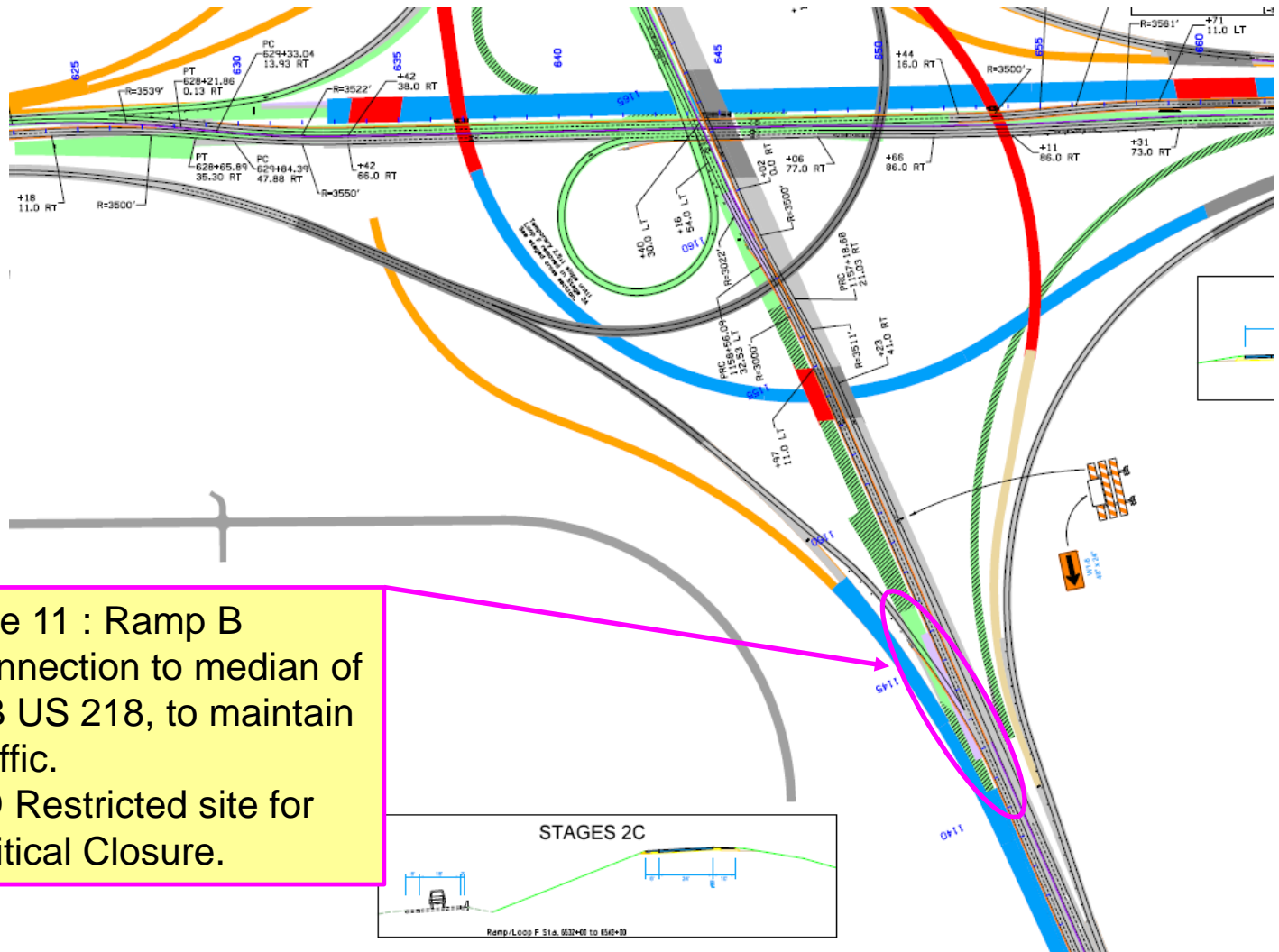


CONTRACT – SITES (REFER TO HANDOUT)



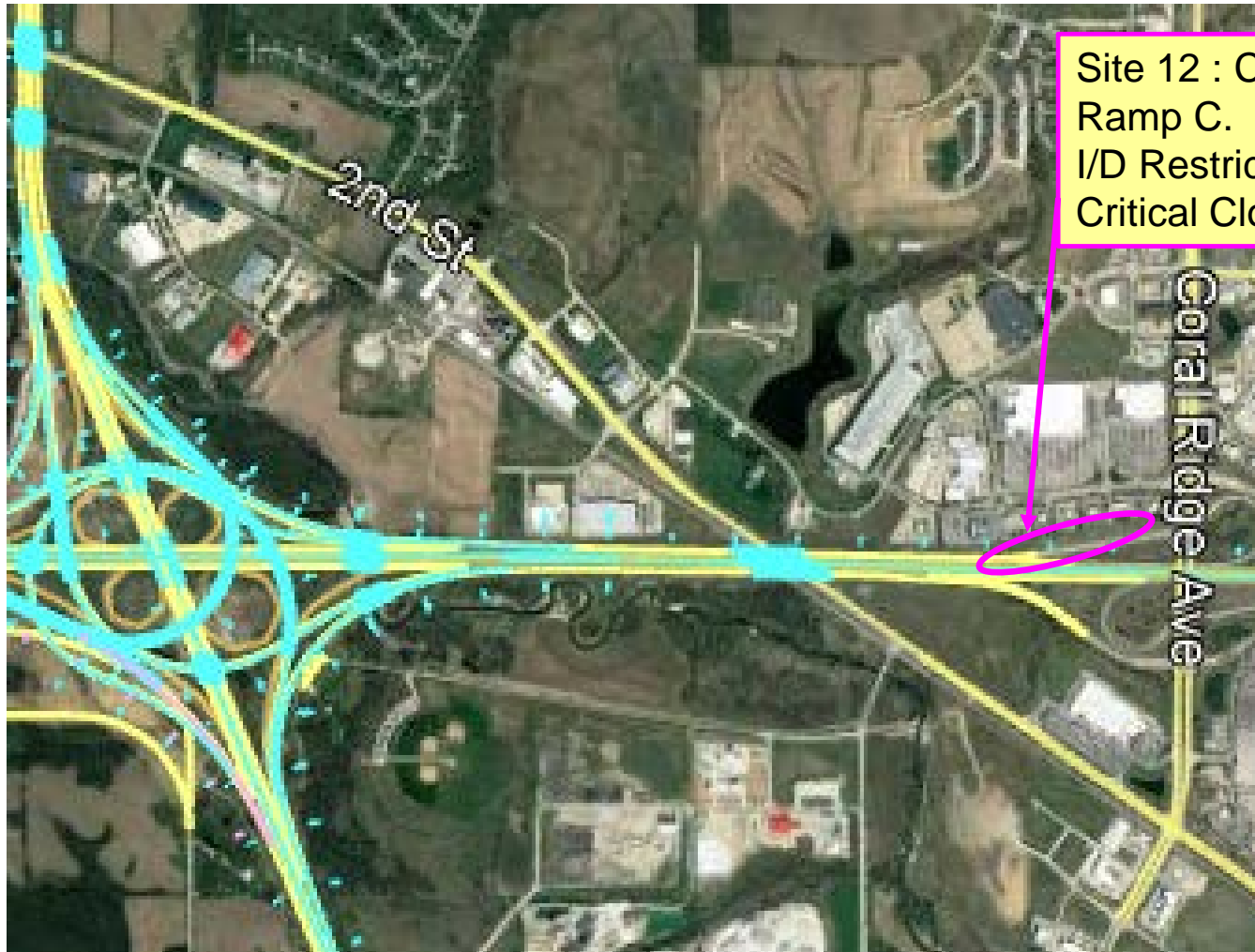
Site 10 : Ireland Ramp
D open to traffic.
I/D Restricted site for
Critical Closure.

CONTRACT – SITES (REFER TO HANDOUT)



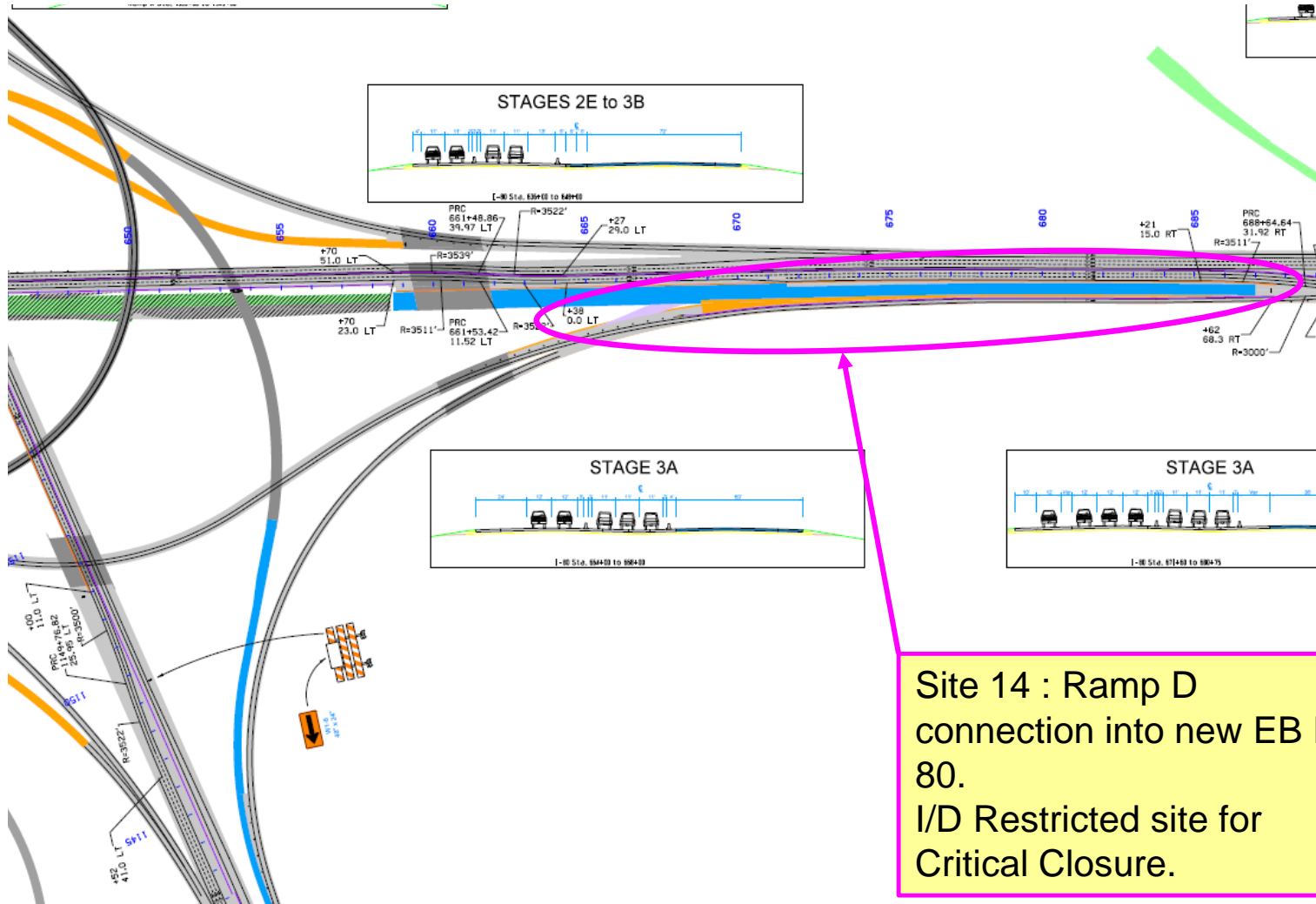
Site 11 : Ramp B connection to median of SB US 218, to maintain traffic.
I/D Restricted site for Critical Closure.

CONTRACT – SITES (REFER TO HANDOUT)



Site 12 : Coral Ridge
Ramp C.
I/D Restricted site for
Critical Closure.

CONTRACT – SITES (REFER TO HANDOUT)



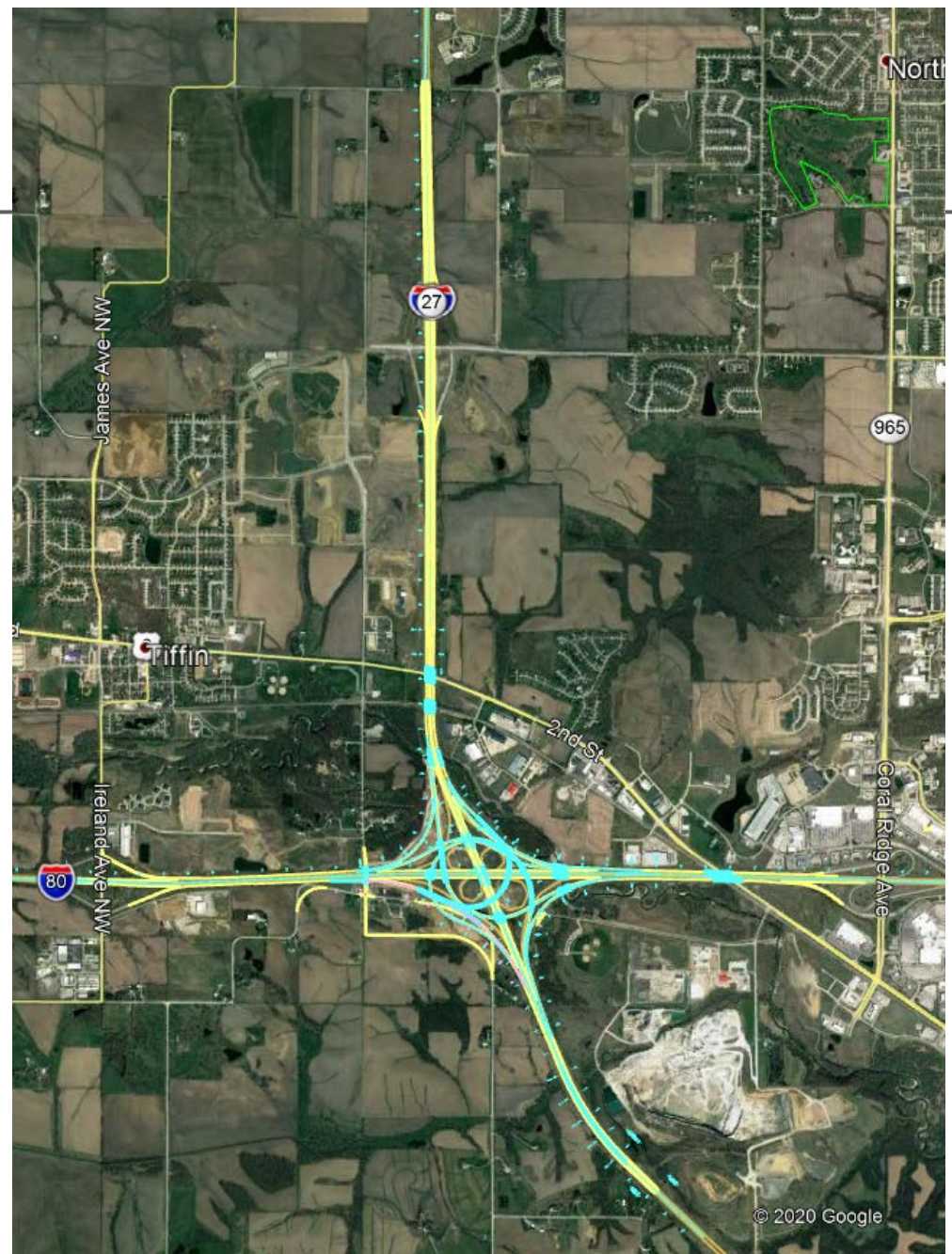
Site 14 : Ramp D
connection into new EB I-
80.
I/D Restricted site for
Critical Closure.

CONTRACT – SITES

Site 00 – Contract Completion
-LD
-Completion Date 7/15/2023
-This site covers all work not covered by the other sites

Work allowed to be completed in 2023 may include:

- Jasper Ave (a.k.a. Park Road) Bridge
- I-80 and I-380 Median Barriers
- Final signing and lighting
- Component pavement markings
- Wall near City Park
- Off-roadway work



SPECIAL PROVISIONS



Matt McLaughlin

Linda Narigon

CONTRACTS – SPECIAL PROVISIONS



- » Contracting Times and Bonuses (SP to be developed)
- » Project Partnering
- » Progress Scheduling
- » e-Builder
- » Railroad – Work on Iowa Interstate RR ROW
- » Instrumentation and Monitoring
- » Mass Concrete
- » Aesthetic Treatment for Concrete Barrier

CONTRACTS – SPECIAL PROVISIONS



- » Cement Treated Subgrade
- » Aesthetic Treatment of MSE Retaining Wall Concrete Panels
- » Girder Erection Plan
- » Anti-Graffiti Coating (at RR – under discussion and consideration to include)
- » Multi-Component Liquid Pavement Markings
- » Preformed Thermoplastic Pavement Markings
- » ITS Infrastructure
- » Lane Rental

CONTRACTS – SPECIAL PROVISIONS

» Lane Rental (Overnight Closures)



Table 1 – Weekday Full Closure Lane Rental Assessment Rates

Time of Day	I-80 Eastbound Closure at Jasper Ave Bridge Demo (\$/hour)	I-80 Westbound Closure at Jasper Ave Bridge Demo (\$/hour)	I-80 Eastbound Closure (\$/hour)
12:00 AM - 1:00 AM	\$500	\$500	\$500
1:00 AM - 2:00 AM	\$500	\$500	\$500
2:00 AM - 3:00 AM	\$500	\$500	\$500
3:00 AM - 4:00 AM	\$500	\$500	\$500
4:00 AM - 5:00 AM	\$500	\$500	\$500
5:00 AM - 6:00 AM	\$1,500	\$1,000	\$4,000
6:00 AM - 7:00 AM	-	-	-
7:00 AM - 8:00 AM	-	-	-
8:00 AM - 9:00 AM	-	-	-
9:00 AM - 10:00 AM	-	-	-
10:00 AM - 11:00 AM	-	-	-
11:00 AM - 12:00 PM	-	-	-
12:00 PM - 1:00 PM	-	-	-
1:00 PM - 2:00 PM	-	-	-
2:00 PM - 3:00 PM	-	-	-
3:00 PM - 4:00 PM	-	-	-
4:00 PM - 5:00 PM	-	-	-
5:00 PM - 6:00 PM	-	-	-
6:00 PM - 7:00 PM	-	-	-
7:00 PM - 8:00 PM	-	-	-
8:00 PM - 9:00 PM	\$3,500	\$2,500	\$7,000
9:00 PM - 10:00 PM	\$2,000	\$1,500	\$5,000
10:00 PM - 11:00 PM	\$2,000	\$1,000	\$3,500
11:00 PM - 12:00 AM	\$500	\$500	\$500

- Prohibited Full Closure Times

CONTRACTS – SPECIAL PROVISIONS



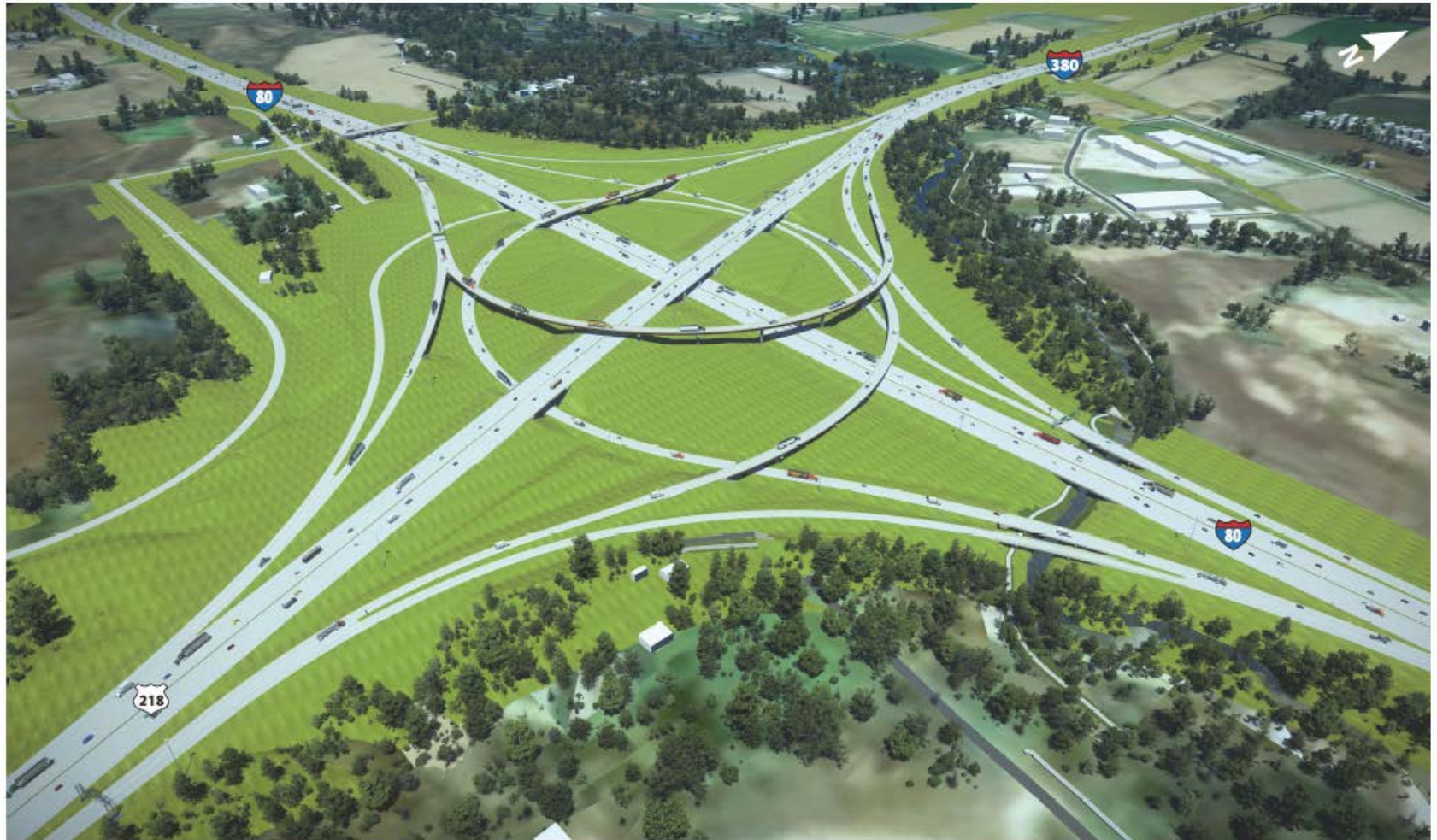
- » Progress Scheduling – planned changes to the SP (compared to last letting)
 - » More frequent updates
 - » ~~Resource~~ Cost Loading 9-Apr-20 correction

CONTRACTS – SPECIAL PROVISIONS

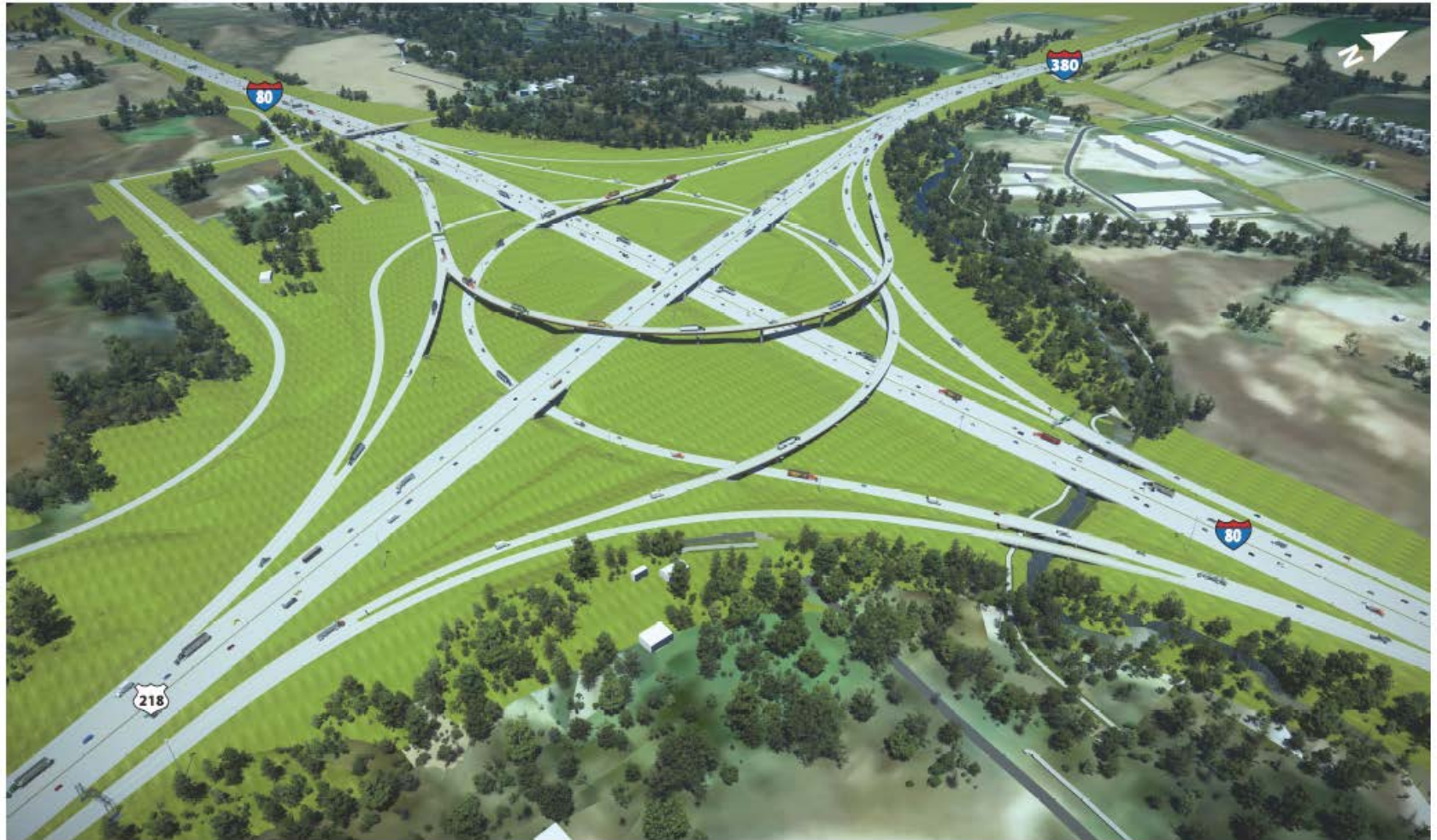


» IAIS RR Agreement and Special Provision

QUESTIONS/FEEDBACK



CLOSING REMARKS





AGC MEETING – MARCH 23, 2020

PROJECT NUMBER IMN-080-6(235)239--0E-52